



PC Members: Nicole Kruckman, Thomas Humphrey, Brad Smith, Melissa Hanson, Bryce Schuenke and Harry Anderson  
City Staff: Community Development Specialist Renee Christianson, Planner Haley Sevensing and City Engineer Rich Revering

**AGENDA**

**TUESDAY, JULY 28, 2020 @ 7:00 PM**

**The City has determined it is not practical or prudent to conduct an in-person Planning Commission meeting due to the local state of emergency and social distancing guidelines. Accordingly, Planning Commission will participate in this meeting via an electronic meeting and the Planning Commission meeting will be conducted under Minnesota Statutes section 13D.021 at the date and time stated above.**

**To the extent practical, members of the public may attend the meeting by utilizing this link: <https://us02web.zoom.us/j/84430970357> or by phone call @ 1-312-626-6799, using meeting ID #844 3097 0357**

- 1. CALL TO ORDER**
- 2. PLEDGE OF ALLEGIANCE**
- 3. APPROVAL OF AGENDA**  
Consider Approval of the Agenda
- 4. PUBLIC COMMENT** (public opportunity to comment on items not listed on the agenda)
- 5. ANNOUNCEMENTS**  
A. None
- 6. APPROVAL OF MINUTES**  
Consider Approval of the following:  
A. May 26, 2020 Minutes
- 7. PUBLIC HEARINGS**  
A. Consider Requests by Kwik Trip, Inc.
  - Request for Rezoning #R2-2020 to Rezone Property from PUD to B5
  - Request for Preliminary Plat Approval of Kwik Trip 1116
  - Request for Conditional Use Permit #C1-2020 to Allow a Convenience Store and Commercial Car Wash
  - Request for Variance #V1-2020 to allow signage on the pump island canopy, and to allow less than the required 24' between fuel pump island curb faces
- 8. GENERAL BUSINESS**  
A. Accessory Dwelling Units
- 9. MISCELLANEOUS**  
A. Tip of the Month – Social Media Policy and Computer Policy  
B. Community Development Updates & Reports  
C. Planning Commission Questions & Comments
- 10. ADJOURNMENT**

**BOARD NOTICE:**

TO DETERMINE IF A QUORUM WILL BE PRESENT, PLEASE CONTACT ELKO NEW MARKET AREA HALL AT 952-461-2777 IF YOU ARE UNABLE TO ATTEND

**PUBLIC NOTICE:**

ANYONE SPEAKING TO THE BOARD SHALL STATE THEIR NAME AND ADDRESS FOR THE RECORD

**MINUTES**  
**CITY OF ELKO NEW MARKET**  
**PLANNING COMMISSION MEETING**  
**May 26, 2020**  
**7:00 PM**

At the start of the meeting, Community Development Specialist Christianson read the following statement into the record: The City has determined it is not practical or prudent to conduct an in-person Planning Commission meeting due to the local state of emergency and social distancing guidelines implemented by Governor Walz. Accordingly, this meeting will be an electronic meeting and conducted under Minnesota Statutes section 13D.021. To the extent practical, members of the public may attend the meeting by utilizing the published link and call in information.

Christianson also stated that due to the electronic meeting format, the meeting would be largely led by City staff, as opposed to being led by the Chair, which is typical. She read allowed the meeting protocol as follows:

- The host will keep all participants muted until the public hearings are opened.
- The Planning Commission will not be muted.
- Staff will specifically solicit question or comments from the Commission and public at various points during each item. Please hold questions and comments until requested.
- If you wish to speak, please “Raise Your Hand” virtually.
- Staff will individually recognize those wishing to speak, and will invite you to speak and unmute your microphone. If you are not being recognized, provide an obvious visual indicator such as raising your hand or waving at the camera. Or, send an email to the Community Development Specialist at rchristianson@ci.enm.mn.us
- Your microphone will be re-muted after you have spoken.
- All votes will be by roll, called by Staff.

She explained for audience members how to “raise their hand” to participate in the meeting on a PC, on a phone or tablet, or if one dialed in by phone. Introductions of the City’s Planning Commission, and City staff were made.

**1. CALL TO ORDER**

Chairman Kruckman called the meeting of the Elko New Market Planning Commission to order at 7:04 p.m.

Commission members present: Smith, Humphrey, Kruckman, Hanson, Schuenke, and Ex-officio member Anderson

Members absent and excused: None

Staff Present: Community Development Specialist Renee Christianson, Planner Haley Sevensing, City Engineer Rich Revering

**2. PLEDGE OF ALLEGIANCE**

Chairman Kruckman led the Planning Commission in the Pledge of Allegiance.

**3. APPROVAL OF AGENDA**

A motion was made by Hanson and seconded by Smith to approve the agenda as submitted.

By Roll Call Vote:

Commissioner Hanson - Aye  
Commissioner Humphrey – Nay  
Commissioner Kruckman – Aye  
Commissioner Schuenke – Aye  
Commissioner Smith - Nay

Motion carried: (5-0)

**4. PUBLIC COMMENT**

There was no public comment.

**5. ANNOUNCEMENTS**

A. None.

**6. APPROVAL OF MINUTES**

A. Chairman Kruckman noted a spelling correction on Page 6 of the draft meeting minutes. It was then moved by Smith and seconded by Schuenke to approve the minutes of the April 28, 2020 meeting with one correction.

By Roll Call Vote:

Commissioner Hanson - Aye  
Commissioner Humphrey – Aye  
Commissioner Kruckman – Aye  
Commissioner Schuenke – Aye  
Commissioner Smith - Aye

Motion carried: (5-0)

**7. PUBLIC HEARINGS**

A. None

**8. GENERAL BUSINESS**

**A. Consider Request for Vacation of Right-of-Way – Mark & Lori Rogers**

Christianson presented the agenda item, which is a request by Mark and Lori Rogers to purchase a portion of City street right-of-way adjacent to their property located at 9711 265<sup>th</sup> Street East. A written request was received by the City which states the purpose of the

request is so the Rogers can build a detached garage on land that is currently platted as City street right-of-way. The Rogers would like to acquire 4,270 square feet adjacent to their property. Christianson explained to the Commission that the City can vacate right-of-way pursuant to Minnesota Statute 412.851, if the City determines that there is no public purpose for the underlying land. Upon vacation of right-of-way, the land automatically reverts to the adjacent property owners.

Christianson explained that staff was seeking preliminary feedback from the Planning Commission which will be forwarded to the City Council for consideration. She displayed a survey of the applicant's property showing the location of the home and property boundaries, noting that there is approximately 28' between the home and west property line.

Christianson explained that there are no existing utilities (sewer/water/stormsewer) within the subject right-of-way. She then explained that staff reviewed the City's Comprehensive Plan to determine if any of the City's adopted plans affect the property being requested for vacation. Both the adopted 2030 Transportation Plan and the Draft 2040 Transportation Plan do depict a future road corridor running north/south in proximity of the right-of-way being requested for vacation. A map showing the overall area was displayed and it was noted that there is a large amount of undeveloped property to the north and east of the subject property. For this reason, staff felt the request to vacate the right-of-way was premature and should not be approved at this time. The Commission provided comment as follows:

- Commissioner Humphrey stated that he was not supportive of approving the request at this time.
- Chairman Kruckman stated that because there is a lot of undeveloped land in the area and there is a potential for development, the right-of-way might be needed in the future.
- Commissioner Smith stated that he is not supportive of vacating the right-of-way at this time.
- Commissioner Hanson stated that she is not supportive of vacating the right-of-way at this time and noted that the property owner seems to be utilizing a portion of the City right-of-way and there needs to be some clean-up at the property.
- Chairman Kruckman stated that she believes it is best to wait, and not vacate the right-of-way.
- Scheunke stated that he is not supportive of vacating the right-of-way because the property owners have ability to construct a garage on their existing property while meeting setback requirements.

The consensus of the Commission was that the City not vacate the requested road right-of-way for the purposes of conveying property to the Rogers for construction of a detached accessory structure for the following reasons:

1. The City's adopted Transportation Plan shows a future road corridor within the Railway Street right-of-way, and therefore the area should be preserved for possible future roadway extensions.

2. There is a significant amount of undeveloped land located to the north and east of the subject property. Until the surrounding property develops into its highest and best use, the City should not consider vacating the right-of-way which may be determined necessary during development of the surrounding property.
3. The property owners have more than 25' located along the westerly side of their existing attached garage to create additional attached garage space.
4. The Rogers have sufficient room on the southerly side of their property to construct a detached accessory structure within setback requirements.

## **B. Housing Presentation**

Sevening presented to the Commission information regarding housing. The presentation covered many key terms and definitions in the affordable housing industry, data regarding the current housing market in the Minneapolis-St. Paul metropolitan area, data regarding income, and an affordable housing “toolbox” with programs and/or policies that the City has implemented, or could be implemented to foster affordable housing. Reviewed in detail was the definition of Area Median Income, and how that definition applies to housing affordability. Sevening reviewed with the Commission the following tools that could be further explored by the City of Elko New Market, as follows:

- Filtering
- Deferment/Assessment of Fees
- Conduit Financing
- Tax Abatement/TIF
- Removal of Regulatory Barriers
- 4D Tax Program
- Water and Sewer Truck Fee Waiver Program
- Accessory Dwelling Units
- Inclusionary Housing
- Funding/Grants

Sevening noted that the City does already have some policies in place regarding deferment of city fees, conduit financing, tax abatement and TIF.

The Commission specifically asked about development and building fees, and how Elko New Market compared with other communities. Christianson reviewed the most recent development fee comparison from 2018 which showed that Elko New Market's fees are somewhat higher than other communities. Revering cautioned the Commission about comparing the City's fees with other communities because every community situation is different.

Smith commented that he is interested in further researching Accessory Dwelling Units and reviewing the Zoning Ordinance to see where changes could be made, and he noted that anything to do with city fees is under the purview of the City Council and not the Planning Commission.

Following significant discussion by the Planning Commission, the Commission recommended that City staff continue to further research all of the tools identified by staff for further review by the Planning Commission.

## **9. MISCELLANEOUS**

### **A. Temporary Outdoor Customer Service Areas**

Christianson advised the Commission that a resolution is being drafted for consideration by the City Council which will allow businesses to establish outdoor customer seating areas, areas for display of products for sale, or other flexibilities to City regulations as determined by the City Administrator, to maximize business operations while complying with the Governor's emergency orders regarding COVID-19. The resolution would temporarily waive most typical City Code requirements for outdoor areas.

### **B. Roundabout Cost Update**

Christianson provided a detailed cost estimate for the CSAH 2 & 91 roundabout, including the City costs, which are estimated at \$1.048 million. She mentioned that the City Council elected to add trails to the project which added approximately \$300,000 to the project, and also decorative lighting which added approximately \$320,000. Both the trail and decorative lighting costs are included in the \$1.048 million City cost.

Kruckman asked what the plan is for the current police station facility. Christianson stated that there are no current plans for re-use of the building. There are minor improvements that will be made to the building as part of the contract to construct the new police station, but a re-use of the building has not been determined.

### **C. Community Development Updates & Reports**

A memorandum containing community development updates was included in the Planning Commission packet. There were no further questions from the Commission about the report.

### **D. Planning Commission Questions and Comments**

There were no further questions or comments from the Commission.

## **10. ADJOURNMENT**

It was moved by Smith and seconded by Humphrey to adjourn the meeting at 8:50 p.m.

By Roll Call Vote:

Commissioner Hanson - Aye  
Commissioner Humphrey – Aye  
Commissioner Kruckman – Aye  
Commissioner Schuenke – Aye

Commissioner Smith – Aye  
Motion carried: (5-0)

Submitted by:



Renee Christianson  
Community Development Specialist

DRAFT



601 Main Street  
Elko New Market, MN 55054  
phone: 952-461-2777 fax: 952-461-2782

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## MEMORANDUM

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**TO:** PLANNING COMMISSION  
STEVE LOWE, KWIK TRIP  
DEAN GEORGE, KWIK TRIP

**FROM:** RENEE CHRISTIANSON, COMMUNITY DEVELOPMENT SPECIALIST  
HALEY SEVEINING, CITY PLANNER

**SUBJECT:** APPLICATION FOR REZONING FROM PUD TO B5, APPLICATION FOR PRELIMINARY AND FINAL PLAT APPROVAL OF KWIK TRIP 1116 CONTAINING 1 LOT ON 4.08 ACRES, APPLICATION FOR CONDITIONAL USE PERMIT TO ALLOW A MOTOR FUEL STATION AND COMMERCIAL CAR WASH, APPLICATION FOR VARIANCE TO ALLOW SIGNAGE ON THE PUMP ISLAND CANOPY AND TO ALLOW LESS THAN THE REQUIRED 24' BETWEEN FUEL PUMP ISLAND CURB FACES

**DATE:** JULY 28, 2020

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PLANNING COMMISSION MEETING: JULY 28, 2020

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CITY COUNCIL MEETING: AUGUST 27, 2020

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60-DAY REVIEW DEADLINE: AUGUST 11, 2020

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120-DAY REVIEW DEADLINE: OCTOBER 10, 2020

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### **Background / History**

The City has received applications from Kwik Trip to develop Outlot A, Elko Market Place 1<sup>st</sup> Addition, which is located at the southwest quadrant of CSAH 2 and France Avenue. Proposed at the site are a 10,950 square foot convenience store, a single bay car wash, a ten-dispenser fueling canopy and two-lane diesel canopy.

Kwik Trip has submitted the following applications:

- Application to rezone the property from Planned Unit Development (PUD) to General Business (B5)
- Application for preliminary and final plat approval of Kwik Trip 1116 containing one lot on 4.08 acres
- Application for conditional use permit allow a motor fuel station and commercial car wash
- Application for variance to allow signage on the pump island canopy
- Application for variance to allow less than the required 24' between fuel pump island curb faces

Kwik Trip has indicated that the project has been identified for construction in 2021. Submitted for review were the following documents:

- Application letter dated June 10, 2020

- Alta Survey, prepared by Sunde Land Surveying and dated October 23, 2019
- Preliminary Plat drawing prepared by Sunde Land Surveying and dated June 8, 2020
- Final Plat drawing prepared by Sunde Land Surveying and undated
- Construction Plans containing 16 sheets, prepared by Carlson McCain and dated May 15, 2020
- Signage Plans containing 12 sheets, prepared by Kwik Trip and dated May 18, 2020
- Building Elevations containing 3 sheets, prepared by Kwik Trip and undated.
- Stomwater Drainage Report (including geotechnical) containing 13 sheets, prepared by Carlson McCain and dated May 15, 2020
- Traffic Impact Study prepared by Kimley Horn, dated April 10, 2020
- Floor Plan

### **Legal Description**

The property proposed for development consists of a platted Outlot currently described as follows:

Outlot A, Elko Market Place First Addition (Parcel ID #23-058001-0)

The current outlot is 4.08 acres in size. The property needs to be platted into a Lot and Block prior to development.

### **Neighborhood Conditions**

- To the north of the property is a rural residential home and undeveloped farm land located outside of the City limits.
- To the east is a large undeveloped property / farm land located within the City limits.
- To the south and west is mostly undeveloped commercial property located within the City limits, with the exception of the Elko New Market Post Office which is located in close proximity. Also located to the south is the Elko Speedway.

The proposed development is generally compatible with the surrounding existing and planned land uses.

### **Comprehensive Land Use Plan**

The City's 2030 Comprehensive Land Use Plan guides the property to a "General Commercial" land use designation. The comprehensive plan contains the following language regarding General Commercial:

*"This classification is characterized by a wide range of commerce, recreation, and entertainment uses whose trade area is the community, and under certain conditions, the region. Arterial roadway oriented uses and single stop or destination stores are included. An important role of this classification is to provide a location for services, goods, and employment opportunities related to the continued dependence upon the automobile for high mobility and to the growth of leisure and recreational activities in the area.*

*Typical Uses: Community retail and service centers, theaters, clubs, and private recreation centers, restaurants, fuel dispensing and the sale of petroleum products, and accessory and related uses that are clearly incidental to the primary use."*

The proposed use of the property as a motor fuel station / car wash meets the intent of the guided land use for the area. The proposed rezoning of the property to General Commercial (B5) meets the intent of the 2030 Comprehensive Plan.

### **Zoning**

Kwik Trip

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The property is currently located in the City limits and zoned Planned Unit Development (PUD). The original PUD zoning for the property, and the terms of the PUD, were approved by the City of Elko on March 17, 2006. At the time of original approval there was a high level concept plan for the proposed commercial area lying north and east of Marketplace Boulevard.

In October of 2008 there was an amendment to the PUD; such amendment related to development of the post office site, a comprehensive sign plan for the area, approval of uses consistent with the B5 zoning district, and required proposed building colors to be subject to City approval.

Kwik Trip has applied to rezone the property to General Business (B5), thereby removing the site from the previously approved PUD development contracts and conditions. The Zoning Ordinance contains the following criteria to be considered when evaluating requests for zoning amendments:

1. The proposed action has been considered in relation to the specific policies and provisions of, and has been found to be consistent with, the official city comprehensive plan.
2. The proposed use is or will be compatible with present and future land uses of the area.
3. The proposed use conforms with all performance standards contained in this title.
4. The proposed use can be accommodated with existing public services and will not overburden the city's service capacity.
5. Traffic generation by the proposed use is within capabilities of streets serving the property.

The proposed B5 zoning is consistent with the adopted Comprehensive Plan which guides the property to General Business. The uses proposed by Kwik Trip would be considered a Convenience Store / Motor Fuel Station and Commercial Car Wash. Both are considered acceptable uses within the B5 zoning district, with an approved Conditional Use Permit. The proposed development is also generally consistent with the original PUD approved layout. Staff supports the request for rezoning the property from PUD to B5 based on the above criteria and because the plan is generally consistent with the originally approved PUD development plan.

### **Lot Size**

The property is currently platted as Outlot A, Elko Market Place First Addition and is 4.08 acres in size or approximately 177,700 square feet. Kwik Trip is proposing to plat the entire property into a single lot, Lot 1 Block 1, Kwik Trip 1116. The minimum lot size in the B5 zoning district is 20,000 square feet and the minimum lot width is 100'. The proposed lot meets the minimum lot size requirements.

### **Setbacks**

The required structure setbacks in the B5 zoning district are 65' from minor arterial roadways (CSAH 2), 65' from collector streets (France Avenue), 20' from a side property line and 30' from a rear property line. Because the lot is a corner lot, the westerly property line would be considered a "rear" property line and requires a 30' setback, and the south property line is considered a "side" property line and requires a 20' setback. The proposed building location meets the minimum building setback requirements.

The motor fuel station canopies must be setback 30' from the front and/or side property lines. Additionally the pump island shall be set back at least 24' from any back of curb and 30' from the property lines. The canopy and pump island shown on the concept plan meets these setback requirements.

### **Height Requirements**

Structures shall not exceed 35' in height in the B5 zoning district. The fuel island canopy shall be not less than 14' and not more than 18' in height. Renderings have been submitted which depict a single story building with a hip roof. The proposed building height is unknown and proposed canopy height is 15.5'. Staff does not anticipate any concerns regarding meeting the building and canopy height requirements.

Kwik Trip

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**Building Design Requirements – Chapter 5 & 26**

Title 11-5-1-A-4(b) of the City Code identifies allowable exterior building materials for commercial zoning districts. Acceptable materials include:

- Brick
- Concrete composite board
- Stone
- Precast concrete panels
- Rock face concrete block
- Wood
- Glass curtain wall panels
- Stucco or EIFS
- Steel
- Aluminum or fiber cement siding

In addition to the above requirements, Title 11-26E-4 contains architectural requirements specific to motor fuel stations, as follows:

1. As a part of the conditional use permit application, a color illustration of all building elevations shall be submitted.
2. The architectural appearance, scale, and functional plan of the building(s) and canopy shall be complementary and compatible with each other and the existing buildings in the neighborhood setting.
3. Exterior wall treatments such as brick, stone (natural or artificial), decorative concrete block and stucco shall be used.

Building elevations / renderings for both the building and fuel canopies have been submitted for the proposed development and are included as Exhibit G. The building is proposed of almost entirely reddish/brown brick with tan accents, and a green metal roof. Staff's opinion is that the building and canopy designs do meet the requirements of Chapters 5 & 26 regarding building design.



**Canopy & Pump Island Design (Chapter 26E-4)**

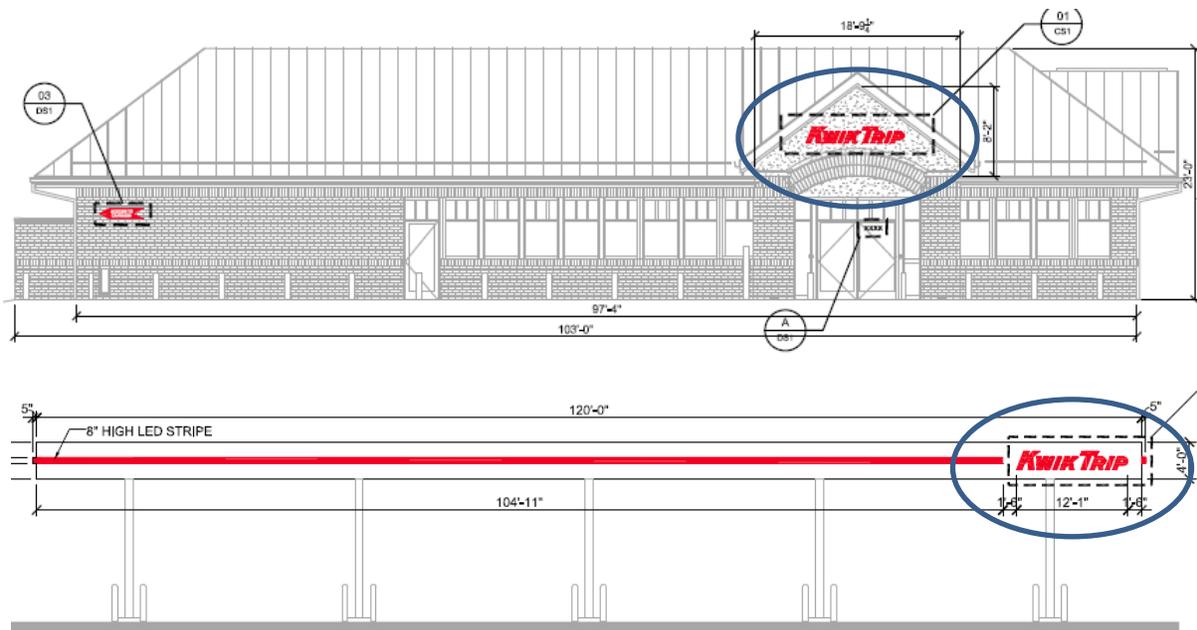
The application material indicates the site will contain 20 vehicle fueling stations and one diesel / truck fueling station. Below are design requirements for canopies and pump islands in the B5 zoning district:

1. The edge of the canopy shall be thirty feet (30') or more from the front and/or side lot line; provided, that adequate visibility both on site and off site is maintained.
2. The canopy shall not exceed eighteen feet (18') in height and shall provide fourteen feet (14') of clearance to accommodate a semitrailer truck passing underneath.
3. The canopy fascia shall not exceed three feet (3') in vertical height.

4. The architectural design, colors, and character of the canopy shall be consistent with the principal building on the site.
5. Signage may be allowed on a detached canopy in lieu of wall signage on the principal structure; provided, that the individual canopy sign does not exceed more than twenty percent (20%) of the canopy facade facing a public right of way.
6. Canopy posts/signposts shall not obstruct traffic or the safe operation of the gas pumps.
7. Pump islands shall be elevated six inches (6") above the traveled surface of the site.
8. All pump islands shall be set at least thirty feet (30') back from any property line. Additionally, the setback between the pump islands' curb face shall be at least twenty four feet (24').

The locations of the two canopies are depicted on the plan and they both meet locational and height requirements. The proposed canopy height is 15.5'. The signage plan is included as Exhibit F and indicates that the canopy fascia is proposed at 4' to be height, which exceeds the 3' maximum vertical height requirements. Staff recommends that the plans be revised to comply with the 3' maximum vertical height requirement for the fuel island canopy fascia.

Signage on the canopy is only permitted under the City code in lieu of signage on the building. The sign plan (Exhibit F) shows that signage is proposed on both the fuel island canopy and the building. The applicant is requesting a variance to allow signage on both the building and fuel island canopy. Staff is supportive of allowing signage on both the building and canopy and would recommend that the Planning Commission/City Council consider amending the City Code in this regard.



The locations of the pump islands are depicted on the plans. The spacing between the pump island curb faces is shown at 21.5' and does not meet the 24' requirement. The applicant is requesting a variance to allow the curb faces to remain at 21.5' apart. In speaking with the applicant's design engineer, he has indicated that all Kwik Trip sites are designed with fuel pump island curb faces spaced at 21.5' apart, and the canopies are structurally designed to meet the same spacing. Kwik Trip is applying for a variance from this requirement so that they do not have to structurally redesign the canopy and because they believe there is sufficient space for vehicles to maneuver within their existing sites. Requiring 24' would add 10' to the fuel island area and canopy. Staff is supportive of allowing 21.5' between the pump island curb faces because all

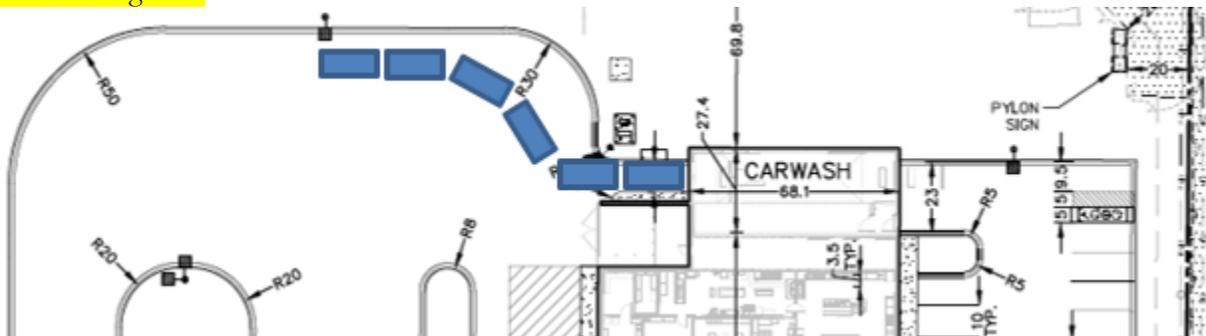
Kwik Trip sites are designed in this fashion and function adequately, and staff recommends that the Planning Commission/City Council consider amending the City Code in this regard.

### **Commercial Car Wash (Chapter 26E)**

The concept plan depicts a single-stall commercial car wash that is attached to the north side of the convenience store building. Commercial car washes are allowed as a conditional use in the B5 zoning district. Commercial car washes have specific performance standards listed in the City Code, as follows:

- A. The architectural appearance and functional plan of the building and site shall not be so dissimilar to the existing buildings or areas as to cause impairment of property values or constitute a blighting influence.
- B. Magazining or stacking space is constructed to accommodate that number of vehicles which can be washed during a maximum thirty (30) minute period and shall be subject to the approval of the City Engineer. *(This would require approximately 6 stacking spaces, and conflicts with another section of the City Code which requires 4 spaces.)*
- C. At the boundaries of a residential district, a strip of not less than five feet (5') shall be landscaped and screened in compliance with section 11-5-4 and chapter 10 of this title.
- D. Parking or car magazine storage space shall be screened from view of abutting residential districts in compliance with section 11-5-4 of this title.
- E. The entire area, other than that occupied by the building or plantings, shall be surfaced with material which will control dust and drainage, which is subject to the approval of the City Engineer.
- F. The entire area shall have a drainage system which is subject to the approval of the City.
- G. All lighting shall be hooded and so directed that the light source is not visible from the public right-of-way or from an abutting residence and shall be in compliance with section 11-4-7 of this title.
- H. Vehicular access points shall be limited, shall create a minimum of conflict with through traffic movement and shall be subject to the approval of the City Engineer.
- I. Provisions are made to control and reduce noise.

There is sufficient vehicle stacking space shown on the plan to meet the requirement for six (6) stacking spaces, and the stacking area does not interfere with normal site circulation. Staff suggests that additional signage and striping be added to the plan to direct customers to the car wash area and to delineate the vehicle stacking area.



### **Off Street Parking – Chapter 9**

Parking requirements can be found in Title 11-9 of the city code. Parking areas must have a concrete curb barrier and be setback 5' from the property line (and entirely outside of easement areas). Parking stalls must be striped with white or yellow paint not less than 4" wide. Parking areas shall be surfaced with concrete, bituminous or pavers. Parking stalls shall be a minimum of 9' x 18' and drive aisles shall be a minimum of 24' in width. Based on the plan submitted, the dimensions of the parking stalls and parking lot drive aisles comply with City code.



Section 11-9-7 (B) of the City Code allows a reduction in the number of required parking spaces to be provided if the applicant can show where the total required parking spaces can be added on the lot, without requiring a variance. The two pink areas shown on the drawing above depict where six additional parking spaces (each) could be added in the future if it were determined that there was a shortage of parking on the site.

### **Off Street Loading – Chapter 9**

Section 11-9-11 of the City Code requires that every retail, industrial or manufacturing, or wholesale use having a gross floor area of six thousand square feet or more shall provide off street loading facilities. Off street loading spaces shall be at least 10' x 25', excluding area for maneuvering vehicles. The concept plan does include an off street loading area on the west side of the building that meets the City Code requirements. The loading area is depicted in cross hatch on the above drawing.

### **Truck Circulation within the Site**

The applicant has provided truck turning movements for a WB-67 which depicts how the vehicle could access the site, including the fuel tank area (for filling tanks), and the diesel pumps and also maneuver in and out of the site. The modeling does depict that the WB-67 could adequately maneuver within the site but does show the truck clipping the northerly curb line as they enter the site at the private drive along France Avenue. Staff recommends that the access / curb be redesigned so that the WB-67 vehicle will not jump the curb as they access the site at the France Avenue / private street entrance.

### **Garbage Dumpster – Chapter 4**

Section 11-4-3 of the City Code requires that all dumpsters, garbage containers, or refuse bins that are stored outside shall be screened from view. Acceptable methods of screening include enclosures made of maintenance free material, brick, or a combination thereof. Gates and doors which allow access to the refuse containers shall have a self-latching mechanism which keeps it closed/locked when not in use. The plans submitted depict a garbage enclosure area that is attached to the north side of the building, with the doors facing to the west. The proposed dumpster enclosure meets the requirements for garbage/refuse.

### **Landscaping – Chapter 10 & 26**

The currently approved PUD does not reference any formally approved landscape plan. The zoning ordinance contains landscaping requirements in Title 11-10 (general landscaping requirements) and Title 11-26A-4 (landscaping requirements for motor fuel stations). Landscaping is not permitted within public rights-of-ways, and is highly discouraged within public drainage and utility easement as these areas are to be reserved for utilities.

Title 11-10-4 contains the following regulations specific to the B5 zoning district:

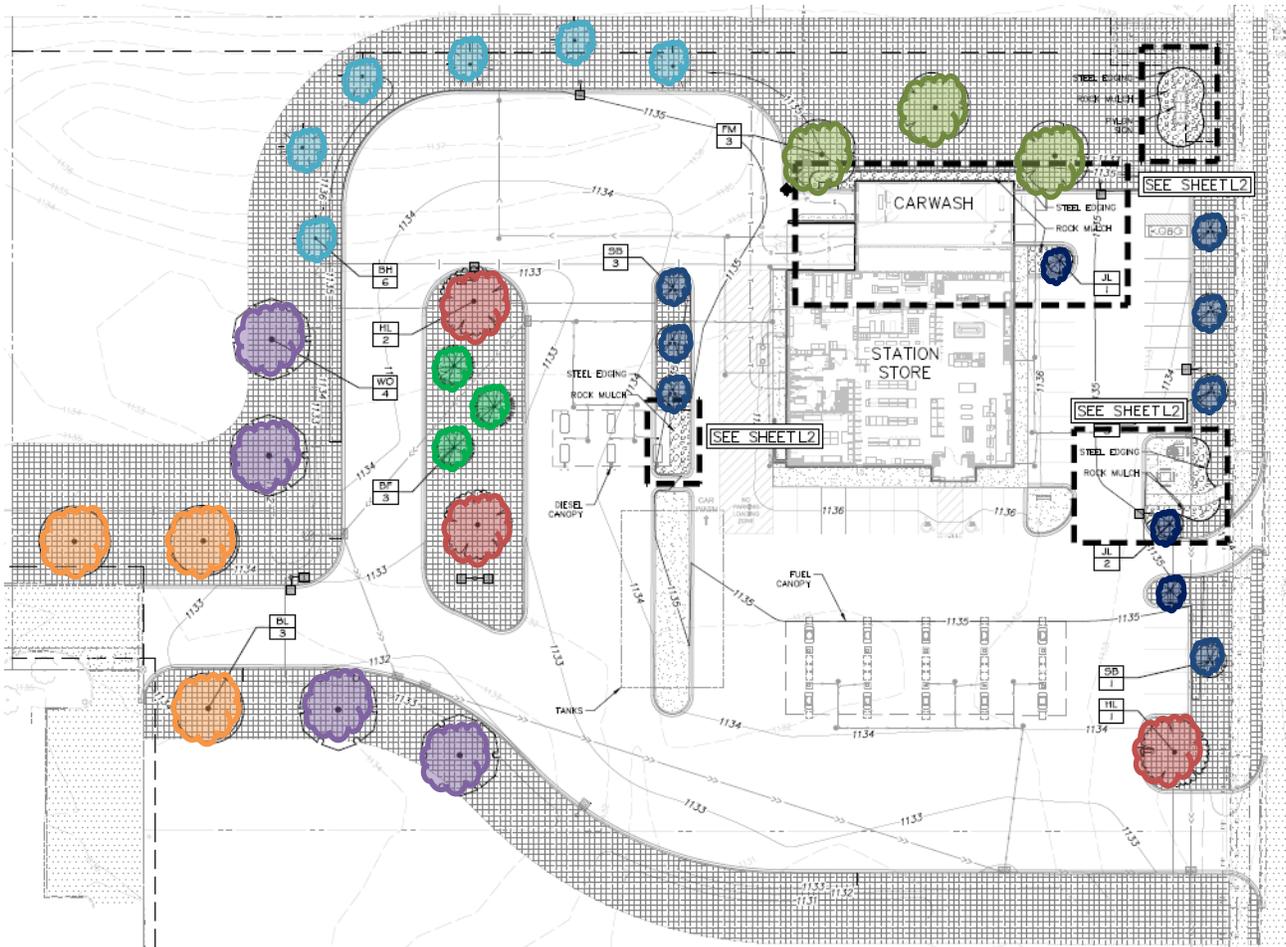
1. At least twenty five percent (25%) of the land area shall be sodded and landscaped with approved ground cover, shrubbery and trees.
2. At least three percent (3%) of the total land area within the parking and driveway areas of a site shall be landscaped. This area is counted as part of the overall required green area.
3. A minimum landscaped buffer area thirty feet (30') in width shall separate any parking, driveway, or structure from a lot line common with any residential district.
4. All properties abutting residential districts shall provide in-ground irrigation systems to all landscaped areas.
5. All areas disturbed by grading which are not built upon, paved or retained as a natural area shall be sodded and/or landscaped unless specifically approved as part of the overall landscape plan.
6. All new plants shall be guaranteed for twelve (12) months from the time planting has been completed. All plants shall be alive, of good quality, and disease free at the end of the warranty

period or be replaced. Any replacements shall be warranted for twelve (12) months from the time of planting.

In addition to the above requirements, Title 11-26E-4 contains landscaping requirements specific to motor fuel stations in the B5 zoning district, as follows:

1. Minimum required green area shall be in accordance with the provisions of chapter 10 of this title.
2. At the boundaries of the lot, the following landscape area shall be required:
  - a. From side and rear property lines, an area of not less than ten feet (10') wide shall be landscaped in compliance with chapter 10 of this title.
  - b. From all road rights of way, an area of not less than fifteen feet (15') wide shall be landscaped in compliance with chapter 10 of this title.
  - c. Where lots abut residentially zoned property, a buffer yard of not less than twenty feet (20') wide shall be landscaped and screened in accordance with chapter 10 of this title.
  - d. The property owner shall be responsible for maintenance of all landscaping, including within the boulevard.

A landscaping plan containing two sheets has been submitted and is included in Exhibit E. **The landscaping plan shows that 43% of the overall site will contain area landscaped with ground cover, shrubbery or trees, and that 8.1% of the area within the parking and driveway areas will be landscaped, meeting ordinance requirements in this regard.**



The landscape plan depicts a variety of trees which are proposed throughout the site. Tree species include:

- Sienna Glen Maple
- Northern Acclaim Thornless Honey Locust
- Swamp White Oak
- Boulevard Linden
- Balsam Fir
- Black Hills Spruce
- Autumn Brilliance Serviceberry
- Ivory Silk Japanese Lilac

In addition to the tree planting schedule outlined above, there are five areas of the site that will contain shrubbery:

- The area on the north side of the car wash, facing CSAH 2
- A parking lot bump-out near the car wash
- The area surrounding the proposed monument sign
- The area surrounding the pedestrian access into the site, from France Avenue
- The raised island near the diesel pumps

The landscape plan as submitted meets the requirements of Title 11-10 and Title 11-26A-4 of the City Code. Staff supports the landscape plan as submitted. The applicant should verify that the proposed landscaping along France Avenue is located outside of drainage and utility easements.

### **Tree Preservation**

Section 12-9-9 of the City's Subdivision Ordinance contains Tree Preservation and Replacement regulations, and requires that 40% of the significant trees must be protected as part of the development. A significant tree is defined as follows:

- A hardwood deciduous tree 6" or greater in diameter
- A softwood deciduous tree 12" or greater in diameter
- A coniferous tree 36" in height or greater

The City has not required a tree inventory be completed as no trees currently exist on the property.

### **Lighting – Chapter 4 & 26**

Title 11-4-7 of the City Code addresses lighting requirements. Under this section, exterior lighting shall not exceed .5 foot-candles at the property line when adjoining residential properties, and 1 foot-candle at the property line when adjoining a similar zone and land use. Any light or combination of lights which casts light on a public street shall not exceed 1 foot-candle (meter reading) as measured from the centerline of said street.

In addition to the above requirements, Title 11-26E-4 of the City Code contains lighting requirements specific to motor fuel stations in the B5 zoning district, as follows:

Exterior Lighting: The lighting shall be accomplished in such a way as to have no direct source of light visible from adjacent residential uses or from the public right of way and shall be in compliance with section 11-4-7 of this title. A comprehensive lighting plan shall be submitted as part of the conditional use permit application and shall be subject to the following performance standards:

1. Canopy Lighting: Canopy lighting under the canopy structure shall consist of canister spotlights recessed into the canopy. No portion of the light source or fixture may extend below the ceiling of

- the canopy. Total canopy illumination below the canopy may not exceed one hundred fifteen (115) foot-candles at ground level
2. Illumination: Maximum site illumination shall not exceed the limits set forth in section 11-4-7 of this title.
  3. Access: Vehicular access points shall create a minimum of conflict with through traffic movement and shall comply with chapter 9 of this title.

A photometric / lighting plan has been submitted and is included in Exhibit E. The plan submitted meets the requirements of Title 11-4-7 and 11-26E-4 except for one small are which aluminate from Pole #5, which has a foot-candle meter reading of 1.1 at the property line. This pole is meant to illuminate the private street running east/west along the south side of the site which will serve access to multiple properties. **Staff supports the lighting plan as submitted.**

**Signage – Chapter 12**

The property is currently zoned PUD and is subject to a master sign plan. The master sign plan approved a pylon sign on the Kwik Trip site which advertises Elko Speedway and the Drive-In Theater. The existing pylon sign is 30’ in height and 24’ in width. **The existing Speedway sign would remain upon development of the Kwik Trip site.** Kwik Trip has requested to rezone to B5 and remove the PUD zoning designation.

A signage plan is included as Exhibit F. Kwik Trip is proposing an additional pylon sign as depicted on Exhibit F, as well as signs on the building, fuel canopies, and directional signs interior to the site. The Kwik Trip pylon sign is proposed at 35’ in height and approximately 13.5’ in width. The pylon sign is proposed at approximately 25’ from the existing Speedway sign. The City code states the following regarding pylon signs.

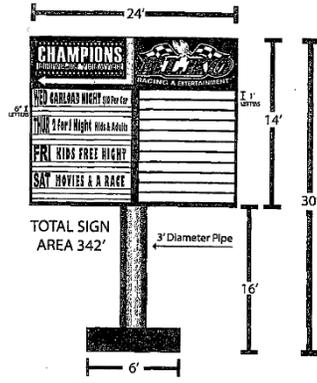
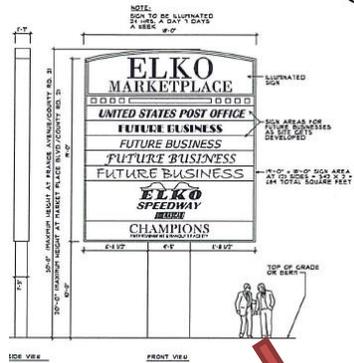
Pylon/Freestanding Signs: One pylon/freestanding sign shall be permitted per street frontage per lot with a maximum of two (2) sides. A sign over seven feet (7’) in height shall be defined as a "pylon/freestanding sign". No pylon/freestanding sign shall extend beyond a property line, building restriction line or right of way line or be located closer than fifty feet (50’) to another freestanding sign. Pylon/freestanding signs may not be located within a clear view triangle area of a corner lot. Setback, size, and height requirements for pylon/freestanding signs shall be as follows:

**PYLON/FREESTANDING SIGNS**

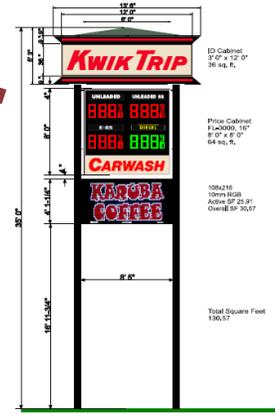
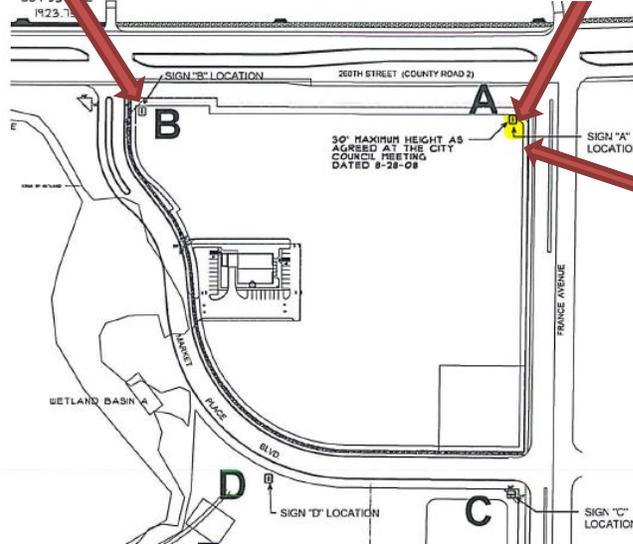
<b>District</b>	<b>Setback<sup>1</sup> (Min.)</b>	<b>Size Per Side (Max.)</b>	<b>Height Max.</b>
B-5 general business	10 ft.	150 sq. ft.	<u>30 ft.</u>

**Staff is recommending that, as a conditional of the CUP, the pylon sign proposed by Kwik Trip be redesigned as a monument sign.** The recommendation is an effort to reduce the visual impact of two pylon signs located so closely together. Staff believes that pylon signs are more appropriate near the interchange commercial areas, but that monument signs have a more appealing visual impact in other commercial areas and throughout the downtown.

As stated earlier in this report, Kwik Trip is also proposing signage on both the fuel island canopy and the building. Allowing signage on both the building and fuel canopy requires a variance, as City Code restricts signage to one or the other. In reviewing the criteria for granting a variance, **staff supports the request for variance because allowing signage on both the building and the fuel island canopy will not alter the essential character of the neighborhood.** Staff further suggests that the Planning Commission consider amending the City Code to allow signage on both canopies and buildings.

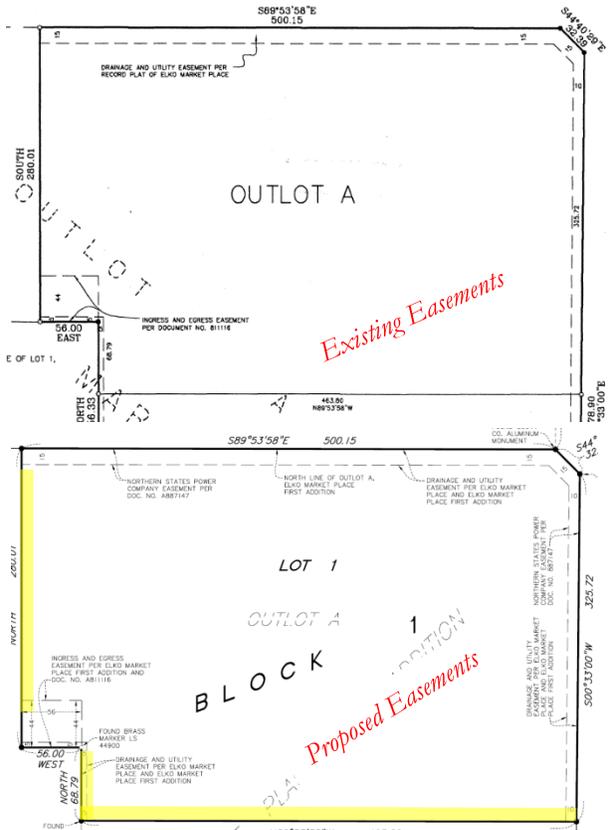


SIGN LOCATION "B"



**Easements**

The City's Subdivision Ordinance (Title 12-9-6(A)) requires that drainage and utility easements shall be provided on all lot lines. At a minimum, these easements shall be 10' wide along all lot lines that abut streets and along all boundaries with land not being developed, and a total of 10' wide on interior property lines. Some easements do already exist along the property boundary as shown on the existing plat drawing. **Based on the ordinance requirements, 10' wide easements should be dedicated along the westerly and southerly property lines as highlighted on the attached drawing.**



**Access / Roads / Transportation Issues**

The proposed development borders two existing roads, County Road 2 and France Avenue. Roads, functional classification of the roads, and comments are as follows:

County Road 2, A Minor Arterial Roadway. The purpose of an A Minor Arterial Roadway is to link urban areas to principal arterials (such as I35) and larger towns. The emphasis of these roadways is mobility as opposed to access. The roadway is under the jurisdiction of Scott County. Scott County does not allow private commercial access connections to the county roadway system. Connections for commercial uses must be made via a public street. No access is proposed to the site from Co Rd 2. There is currently a total of 200’ of right-of-way along CSAH 2 in this location.

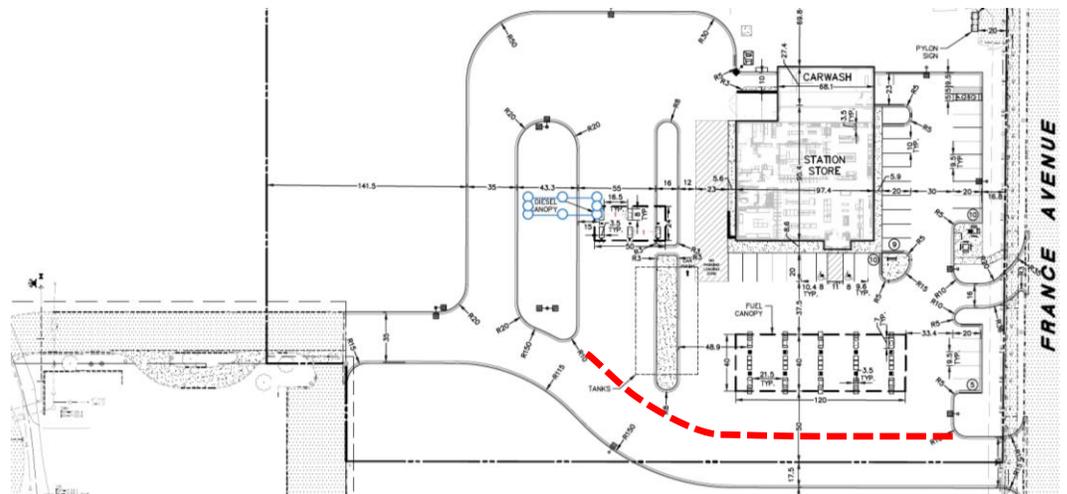
Official comments from Scott County had not been received at the time of this report. Staff does not anticipate any concerns from Scott County based on their review of a concept plan in March of 2020.

France Avenue, Major Collector. The purpose of a major collector is typically to link neighborhoods together within a city or link neighborhoods to business concentrations. Access to collector roadways should be made via local streets and private access should be avoided. For commercial / industrial lots that front on an arterial roadway or major collector, the Subdivision Ordinance states that lots shall be designed to minimize the number of access points onto the roads by using access from a local road, a frontage road, frontage driveways with cross easements serving multiple properties, shared driveways, or as a last resort one driveway access no more than 200’ to another driveway.

There are two proposed accesses into the site from France Ave. The southerly access is proposed as a private drive serving multiple properties. The southerly access would connect to the existing private drive on the north side of the post office, and would allow full circulation through the commercial area from Marketplace Boulevard to France Avenue. This private drive / thru street is consistent with original concept plans for the commercial area and was permitted as part of the original PUD approvals. A shared access maintenance agreement will be needed to memorialize roles and responsibilities related to maintenance of this shared private drive.

The northerly access is proposed as a private access directly into the Kwik Trip site that would be a “right-in” only; no exiting would be allowed from this point. Because private driveways are not recommended on Major Collector Streets, and based on the projected traffic volumes from the site, a Traffic Impact Study was requested by the City. A Traffic Impact Study prepared by Kimley Horn, dated April 10, 2020, has been submitted to the City. The City Engineer and Scott County have reviewed the Traffic Impact Study and have no objections to the proposed right-in access based on the findings contained in the Study.

Staff suggests that some type of pavement markings be added to the plans to help delineate the private drive “thru movement” from the fuel pumping area (see drawing).

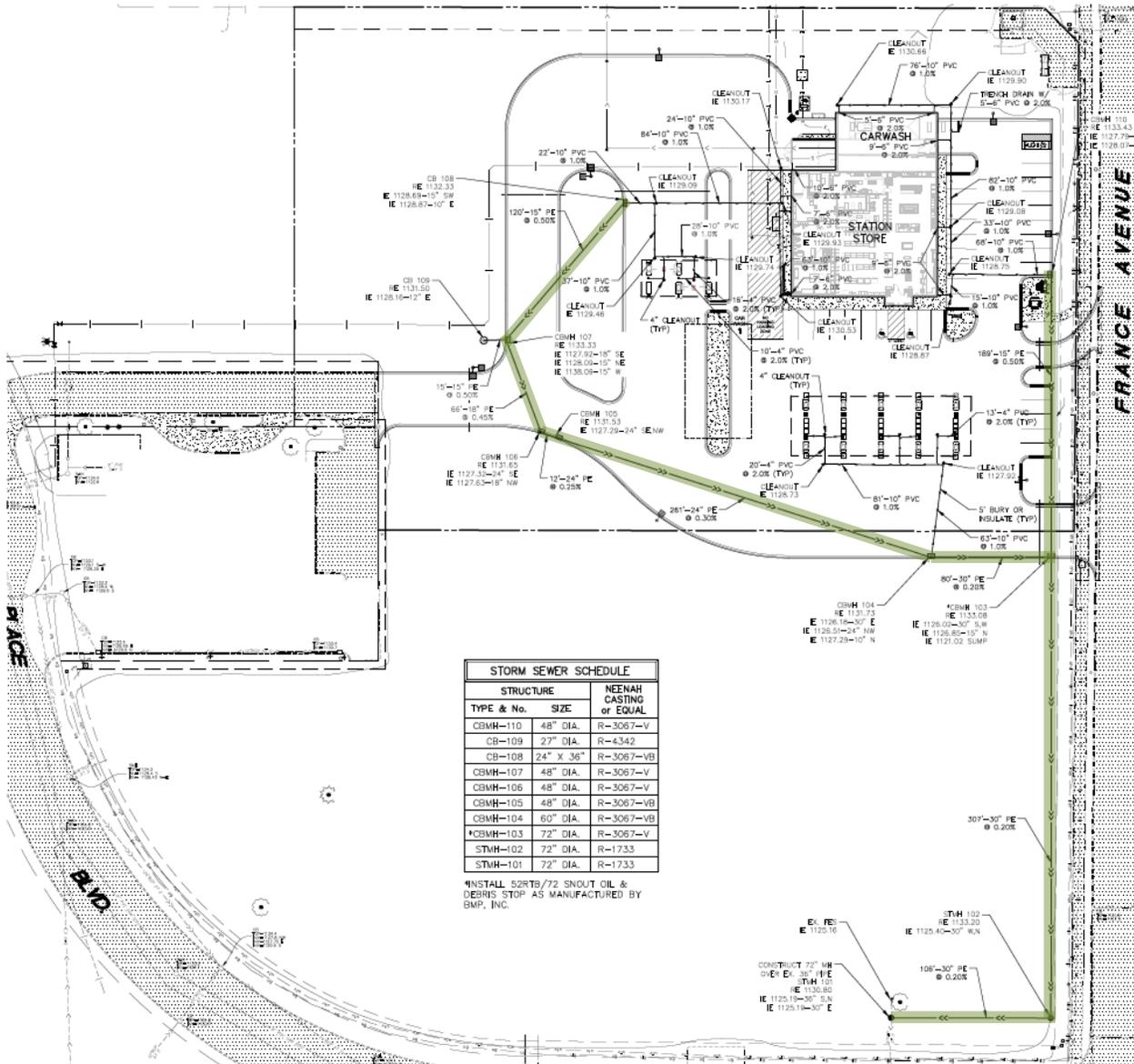


**Wetlands / Floodplain / DNR Protected Waters**

There are no known wetlands, floodplains, or DNR Protected Waters on the site.

**Stormwater**

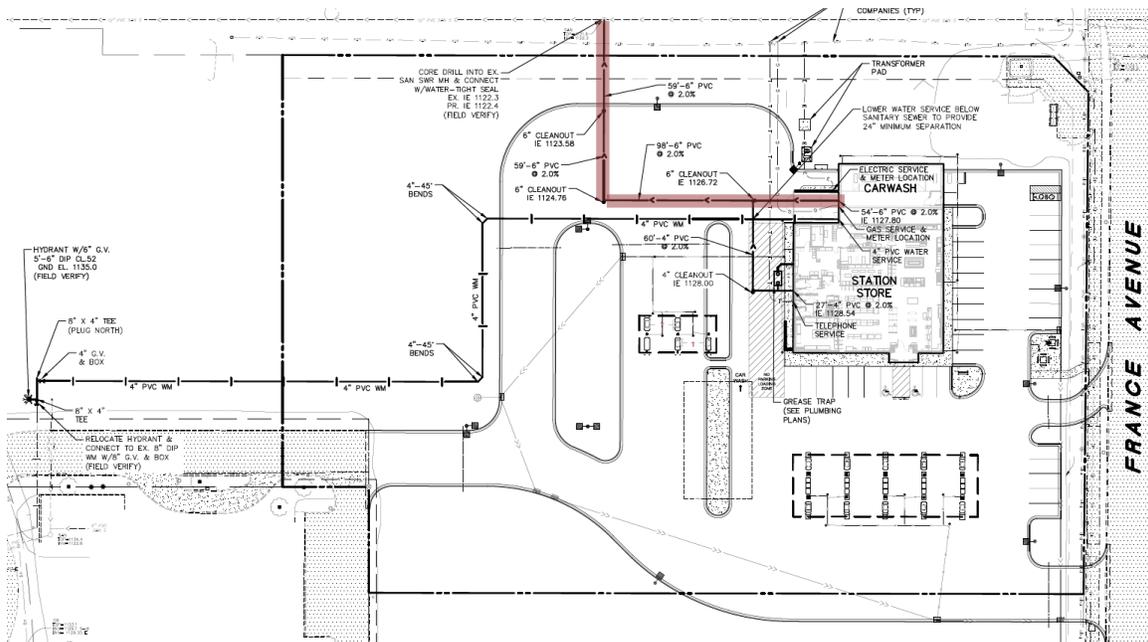
The subject property lies entirely within the Vermillion Watershed District. The grading and utility plan depicts surface water being collected from the site, draining through the parcel to the south, and ultimately into the existing stormwater pond located at the southwest corner of Marketplace Blvd and France Ave. The pond was designed to handle stormwater from the subject property. The proposed stormsewer line running across Outlot B, Elko Marketplace 1<sup>st</sup> Addition is outside of any public drainage and utility easements. Evidence of a private easement allowing the stormsewer line to be located on Outlot B must be provided to the City prior to construction. The stormsewer improvements are considered private and will not be maintained by the City. The City Engineer’s comments regarding stormsewer are contained in a memo dated 7.17.20.



Stormsewer Map

## Sanitary Sewer

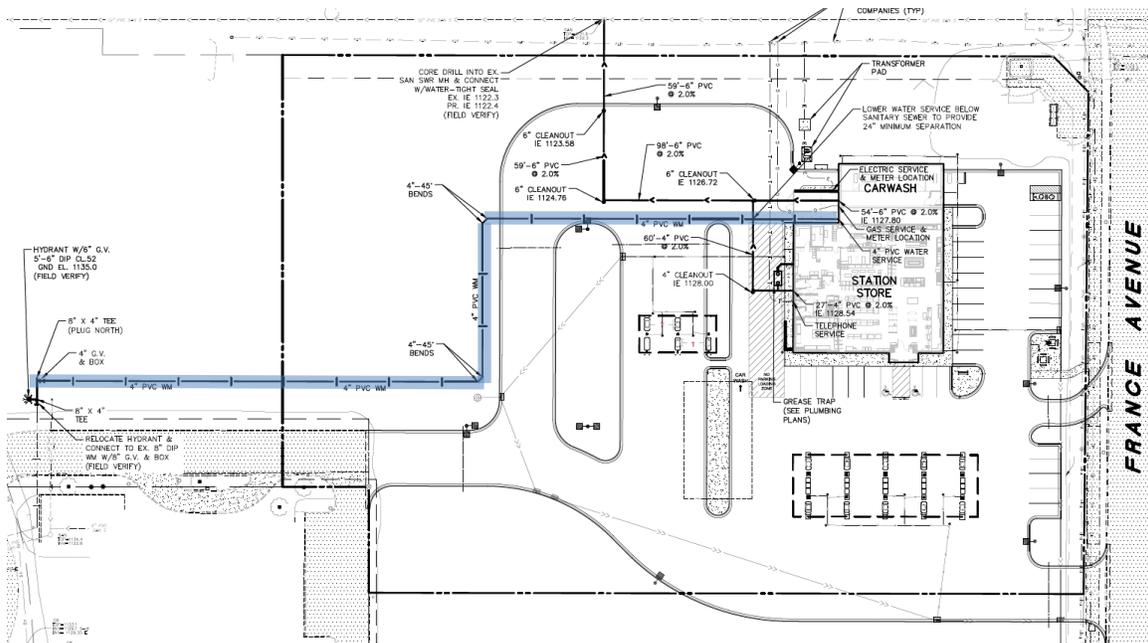
There is an existing 6" PVC sanitary sewer forcemain in France Avenue and within the southerly CSAH 2 right-of-way. There is also an existing 8" PVC gravity sewer line located to the west along Market Place Blvd. The utility plan submitted depicts that the applicant is proposing to serve the Kwik Trip site by connecting to the existing gravity sewer line located along CSAH 2. **The sanitary sewer improvements are considered private and will not be maintained by the City.** The City Engineer's comments regarding sanitary sewer are contained in a memo dated 7.17.20. **The Public Works Director is recommending that the sewer be connected to the sewer main, rather than the manhole as depicted on the plans.**



*Sanitary Sewer Map*

## Water

There is an existing water line located to the west of the Kwik Trip site. The utility plan depicts that the applicant is proposing to serve the Kwik Trip site by connecting to the existing water line located on the New Market Bank property and extending it east to their site. **To construct the water line as proposed, proof of easement through the New Market Bank property will be needed.** The water line is considered private and will not be maintained by the City. Additional hydrants may be recommended by the Building Official or Fire Chief. Should an additional fire hydrant be required, the hydrant will be considered private but the City will do annual maintenance on the fire hydrant (flushing, etc.).



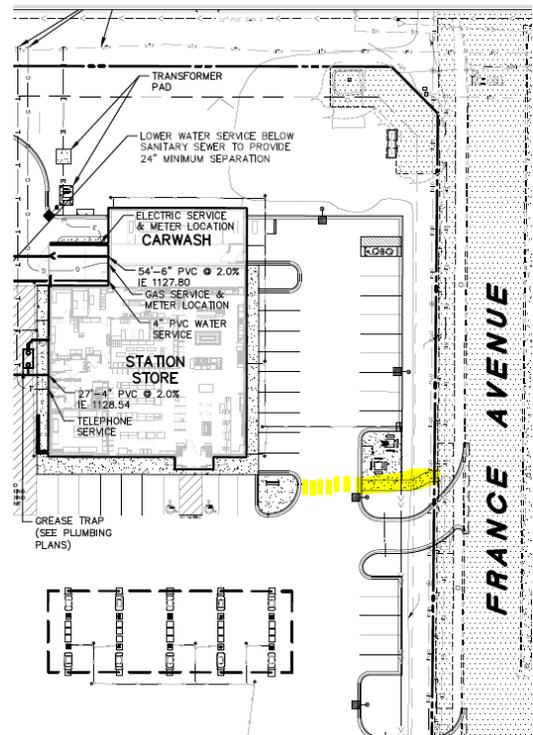
Water Map

**Sidewalks & Trails**

The City’s 2030 Park & Trail Plan identifies a proposed sidewalk / trail corridor along the south side of CSAH 2. The City’s Transportation Plan also recommends that sidewalks or trails be constructed adjacent to all minor collectors, major collectors, and minor arterial roadways. An 8’ trail exists on the west side of France Avenue. The City and County are constructing a 10’ trail along the south side of CSAH 2 as part of the roundabout project. This will make the proposed development more accessible to bikers and walkers.

The plan depicts pedestrian access to the site by connecting a small segment of sidewalk to the existing France Avenue trail. One suggestion would be to include pavement markings or colored concrete across the drive aisle to further delineate the pedestrian access into the building.

Section 11-9-8(H)(3) of the City Code requires sidewalks abutting parking stalls be 6’ in width.



**Parks Commission Comments**

Title 12-9-8 of the City Code requires that 10% of the gross area being platted be dedicated for public parks, trails, or open space. If no land dedication is required, the park fee is \$2,800 per acre for commercial development. The park dedication obligations were satisfied during the original platting of the area and outlined in the PUD Agreement and Development Contract of Elko Market Place dated March 17, 2006. Therefore, there is no need for Parks Commission input during the development process.

### **City Engineer Comments**

The City Engineer has reviewed the submittal and has provided comment in a memorandum dated July 17, 2020 and attached as Exhibit J.

### **Public Works Director Comments**

The Public Works Director has reviewed the plan submittal. He understands that the water line, sanitary sewer line, and storm sewer line serving the proposed development are all considered private lines. He has also recommended that the sanitary sewer be tied into the existing trunk sewer line rather than the manhole.

### **Fire Chief Comments**

The Fire Chief has not commented on the plan. An emergency / rapid access system will be required on the proposed commercial building at the time of construction (Knox Box).

### **Police Chief Comments**

The Police Chief has reviewed the plan submittal and recommends that a sign be installed at the northerly entrance into the site stating, "vehicles only / no semi's".

### **Building Official Comments**

At the time of this report, the Building Official had not reviewed the plans.

### **City Attorney Comments**

At the time of this report, the City Attorney had not reviewed the plans.

### **Scott County Highway Department Comments**

At the time of this report, comments from Scott County had not yet been received. Several comments previously made by Scott County are referenced in the transportation section of this report, and the most recent comments from Scott County are attached and dated August 8, 2016.

### **DNR Comments**

Comments were not solicited from the DNR.

### **Staff Recommendation:**

Staff recommends approval of the request to **rezone** the property from PUD to B5 for the following reasons:

1. The proposed rezoning has been considered in relation to the specific policies and provisions of, and has been found to be consistent with, the official City Comprehensive Plan which guides the property to a commercial land use.
2. The proposed commercial use of the property for a convenience store / motor fuel station and commercial car wash is compatible with present and future land uses of the area.
3. The proposed use of the property complies with performance standards contained in the City's Zoning & Subdivision Ordinances, except as specifically identified in requested variances.
4. The proposed commercial use of the property can be accommodated with existing public services and will not overburden the City's service capacity.
5. Traffic generation by the proposed development is within capabilities of existing streets serving the property.

Staff recommends approval of the request for **preliminary plat approval** of Kwik Trip 1116 containing one lot on 4.08 acres for the following reasons:

- 1) The proposed plat complies with the purposes of the Zoning Ordinance and Comprehensive Plan.

And subject to the following conditions:

- 1) Preliminary plat approval is granted in accordance with the following plans, except as otherwise noted in the conditions below:
  - a. Preliminary Plat drawing prepared by Sunde Land Surveying and dated June 8, 2020
  - b. Final Plat drawing prepared by Sunde Land Surveying and undated
  - c. Stormwater Drainage Report (including geotechnical) containing 13 sheets, prepared by Carlson McCain and dated May 15, 2020
- 2) The developer must enter into a development contract with the City of Elko New Market at the time of final plat approval.
- 3) The plat shall be revised to include 10' drainage and utility easements on the westerly and southerly property lines.
- 4) Approval is subject to the recommendations and approvals of the City Engineer, Public Works Director and City Attorney.

Staff recommends approval of the request for **conditional use permit** to allow a Convenience Store / Motor Fuel Station and Commercial Car Wash for the following reasons:

1. The proposed use of the property as a convenience store / motor fuel station and commercial car wash is consistent with the purpose and intent of comprehensive plan and the B5 zoning district.
2. The proposed development conforms with all applicable performance standards, including performance standards for motor fuel stations and commercial car washes as stated in Section 11-26E-4 of the City Code, except as specifically identified in requested variances.
3. The proposed development can be accommodated by existing public services.
4. Traffic generation by the proposed convenience store / motor fuel station and commercial car wash is within the capabilities of the existing streets serving the property, which has been documented in a Traffic Impact Study prepared by Kimley Horn and dated April 10, 2020.

And with the following conditions:

1. Approval is granted in accordance with the following plans, except as otherwise noted in the conditions below:
  - a. Preliminary Plat drawing prepared by Sunde Land Surveying and dated June 8, 2020
  - b. Final Plat drawing prepared by Sunde Land Surveying and undated
  - c. Construction Plans containing 16 sheets, prepared by Carlson McCain and dated May 15, 2020
  - d. Signage Plan containing 12 sheets, prepared by Kwik Trip and dated May 18, 2020
  - e. Building Elevations containing 3 sheets, prepared by Kwik Trip and undated
  - f. Stormwater Drainage Report (including geotechnical) containing 13 sheets, prepared by Carlson McCain and dated May 15, 2020
  - g. Traffic Impact Study prepared by Kimley Horn, dated April 10, 2020
  - h. Floor Plan, undated
2. The applicant shall comply with recommendations contained in the City Engineer's memo dated July 17, 2020, and the recommendations of the Public Works Director and City Attorney.
3. The plan shall be revised to show the sanitary sewer line be tied into the existing trunk sewer line rather than the sanitary manhole.
4. If parking on the site becomes overburdened, the applicant agrees to construct additional parking on the site to meet the minimum city code requirements of 62 parking spaces.

5. The access / curb shall be redesigned so that a WB-67 truck will not jump the curb as they access the site at the France Avenue / private drive entrance.
6. Trees shall be located outside of drainage and utility easements.
7. The proposed pylon sign shall be redesigned as a monument sign.
8. Evidence of a private easement allowing the proposed stormsewer line to be located on Outlot B must be provided to the City prior to construction.
9. Evidence of a private easement allowing the proposed water line to be located on the New Market Bank property must be provided to the City prior to construction.
10. The plans shall be revised to include additional fire hydrants, if recommended by the building official or fire chief.
11. The plans shall be revised to include pavement markings or colored concrete across the drive aisle to further delineate the pedestrian access into the building.
12. The plans shall be revised to include signage and pavement markings that direct customers to, and clearly delineate, the car wash stacking area.
13. An emergency / rapid access system (Knox Box) will be required on the building at the time of construction.
14. The sign plan shall be amended to add a sign at the northerly entrance into the site stating “vehicles only / no semi’s”.
15. The plans shall be revised to comply with the 3’ maximum vertical height requirement for the fuel island canopy fascia.
16. A shared access maintenance agreement will be needed to memorialize roles and responsibilities related to maintenance of this shared private drive.
17. Pavement markings shall be added to the plans to help delineate the private drive “thru movement” from the fuel pumping area.

And noting that:

1. The stormsewer, sanitary sewer, and water improvements are considered private and will not be maintained by the City.

Staff recommends approval of the request for **variance** to allow signage on the pump island canopy and to allow less than 24’ between fuel pump island curb faces for the following reasons:

1. The variance requests will not alter the essential character of the neighborhood.
2. The applicant is proposing to use the property in a reasonable manner not permitted by City Code.

**Attachments:**

Variance Criteria

Rezoning Criteria

Conditional Use Permit Criteria

Ordinance requirements for commercial car washes

Ordinance requirements for motor fuel stations

Exhibit A – Applicant’s letter dated June 10, 2020

Exhibit B – Alta Survey, prepared by Sunde Land Surveying and dated October 23, 2019

Exhibit C – Preliminary Plat drawing prepared by Sunde Land Surveying and dated June 8, 2020

Exhibit D – Final Plat drawing prepared by Sunde Land Surveying and undated

Exhibit E – Construction Plans containing 16 sheets, prepared by Carlson McCain and dated May 15, 2020

Exhibit F – Signage Plan containing 12 sheets, prepared by Kwik Trip and dated May 18, 2020

Exhibit G – Building Elevations containing 3 sheets, prepared by Kwik Trip and undated

Exhibit H – Stormwater Drainage Report (including geotechnical) containing 13 sheets, prepared by Carlson McCain and dated May 15, 2020

Exhibit I – Traffic Impact Study prepared by Kimley Horn, dated April 10, 2020

Exhibit J – Floor Plan, undated

Exhibit K – City Engineer review memorandum dated July 17, 2020

Kwik Trip

7/28/20

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## **Rezoning Criteria**

1. The proposed action has been considered in relation to the specific policies and provisions of, and has been found to be consistent with, the official city comprehensive plan.
2. The proposed use is or will be compatible with present and future land uses of the area.
3. The proposed use conforms with all performance standards contained in this title.
4. The proposed use can be accommodated with existing public services and will not overburden the city's service capacity.
5. Traffic generation by the proposed use is within capabilities of streets serving the property.

## Variance Criteria

The board of adjustments and appeals shall not approve any variance request unless they find that failure to grant the variance will result in practical difficulties. The following criteria must also be met:

- 1) That the variance would be consistent with the comprehensive plan.
- 2) That the variance would be in harmony with the general purpose and intent of this title.
- 3) That the purpose of the variance is not based exclusively upon economic considerations.
- 4) That the plight of the landowner is due to circumstances unique to the property not created by the landowner.
- 5) That the granting of the variance will not alter the essential character of the neighborhood in which the parcel of land is located.
- 6) That the property owner proposes to use the property in a reasonable manner not permitted by this title.
- 7) That the requested variance is the minimum action required to eliminate the practical difficulty.
- 8) That the proposed variance does not involve a use that is not allowed within the respective zoning district.

"Practical difficulties", as used in connection with the granting of a variance, means that:

- 1) The property owner proposes to use the property in a reasonable manner not permitted by the zoning ordinance; and
- 2) The plight of the landowner is due to circumstances unique to the property not created by the landowner; and
- 3) The variance, if granted, will not alter the essential character of the locality.

Economic considerations alone shall not constitute practical difficulties. Practical difficulties include, but are not limited to, inadequate access to direct sunlight for solar energy systems. (Ord. 97, 6-28-2012)

### **State Law Pertaining to Variance Requests**

Minnesota Statute 462.357 Subd. 6 provides a mechanism for cities to grant variance requests when "practical difficulties" have been determined. Under the statutory practical difficulties standard, a landowner is entitled to a variance if the facts satisfy the three-factor test of (1) reasonableness, (2) uniqueness, and (3) essential character.

- 1) Reasonableness. Does the landowner propose to use the property in a reasonable way but cannot do so under the provisions of the ordinance?
- 2) Uniqueness. Is the landowner's problem due to circumstances unique to the property not caused by the landowner? The uniqueness generally relates to the physical characteristics of the piece of property and economic considerations alone cannot create practical difficulties.
- 3) Essential Character. Will the variance, if granted, alter the essential character of the neighborhood? Would the resulting structure be out of scale, out of place, or otherwise inconsistent with the surrounding area?

## Conditional Use Permit Criteria – General

The planning commission shall consider possible effects of the proposed conditional use. Its judgment shall be based upon, but not limited to, the following factors:

1. The proposed action has been considered in relation to the specific policies and provisions of and has been found to be consistent with the official city comprehensive plan.
2. The proposed use is or will be compatible with present and future land uses of the area.
3. The proposed use conforms with all performance standards contained in this title.
4. The proposed use can be accommodated with existing public services and will not overburden the city's service capacity.
5. Traffic generation by the proposed use is within capabilities of streets serving the property.

As may be applicable, the evaluation of any proposed conditional use permit request shall be subject to and include, but not be limited to, the following general performance standards and criteria:

1. The use and the site in question shall be served by a street of sufficient capacity to accommodate the type and volume of traffic which would be generated and adequate right of way shall be provided.
2. The site design for access and parking shall minimize internal as well as external traffic conflicts and shall be in compliance with chapter 9 of this title.
3. If applicable, a pedestrian circulation system shall be clearly defined and appropriate provisions made to protect such areas from encroachment by parked or moving vehicles.
4. Adequate off street parking and off street loading shall be provided in compliance with chapter 9 of this title.
5. Loading areas and drive-up facilities shall be positioned so as to minimize internal site access problems and maneuvering conflicts, to avoid visual or noise impacts on any "adjacent" residential use or district, and provided in compliance with chapter 9 of this title.
6. Whenever a nonresidential use is "adjacent" to a residential use or district, a buffer area with screening and landscaping shall be provided in compliance with chapter 10 of this title.
7. General site screening and landscaping shall be provided in compliance with chapter 10 of this title.
8. All exterior lighting shall be so directed so as not to cast glare toward or onto the public right of way or neighboring residential uses or districts, and shall be in compliance with section [11-4-7](#) of this title.
9. Potential exterior noise generated by the use shall be identified, and mitigation measures, as may be necessary, shall be imposed to ensure compliance with section [11-4-8](#) of this title.
10. The site drainage system shall be subject to the review and approval of the city engineer.
11. The architectural appearance and functional design of the building and site shall not be so dissimilar to the existing or potential buildings and area so as to cause a blighting influence. All sides of the principal and accessory structures are to have essentially the same or coordinated, harmonious exterior finish materials and treatment.
12. All signs and informational or visual communication devices shall be in compliance with chapter 12 of this title.
13. The use and site shall be in compliance with any federal, state or county law or regulation that is applicable, and any related permits shall be obtained and documented to the city.
14. Any applicable business licenses mandated by this code are approved and obtained.
15. The hours of operation may be restricted when there is judged to be an incompatibility with a residential use or district.
16. The use complies with all applicable performance standards of the zoning district in which it is located, and where applicable, any nonconformities shall be eliminated.
17. All additional conditions pertaining to a specific site are subject to change when the council, upon investigation in relation to a formal request, finds that the general welfare and public betterment can be served as well or better by modifying or expanding the conditions set forth herein.

## Conditional Use Permit Criteria for Commercial Car Washes

### Commercial car washes (drive-through and self-service); provided, that:

- A. The architectural appearance and functional plan of the building and site shall not be so dissimilar to the existing buildings or areas as to cause impairment of property values or constitute a blighting influence.
- B. Magazining or stacking space is constructed to accommodate that number of vehicles which can be washed during a maximum thirty (30) minute period and shall be subject to the approval of the City Engineer.
- C. At the boundaries of a residential district, a strip of not less than five feet (5') shall be landscaped and screened in compliance with section 11-5-4 and chapter 10 of this title.
- D. Parking or car magazine storage space shall be screened from view of abutting residential districts in compliance with section 11-5-4 of this title.
- E. The entire area, other than that occupied by the building or plantings, shall be surfaced with material which will control dust and drainage, which is subject to the approval of the City Engineer.
- F. The entire area shall have a drainage system which is subject to the approval of the City.
- G. All lighting shall be hooded and so directed that the light source is not visible from the public right-of-way or from an abutting residence and shall be in compliance with section 11-4-7 of this title.
- H. Vehicular access points shall be limited, shall create a minimum of conflict with through traffic movement and shall be subject to the approval of the City Engineer.
- I. Provisions are made to control and reduce noise.

## Conditional Use Permit Criteria for Motor Fuel Stations

### Motor fuel stations; provided, that:

- A. Motor Fuel Facilities: Motor fuel facilities shall be installed in accordance with state and city standards. Additionally, adequate space shall be provided to access fuel pumps and allow maneuverability around the pumps. Underground fuel storage tanks are to be positioned to allow adequate access by motor fuel transports and unloading operations which do not conflict with circulation, access and other activities on the site. Fuel pumps shall be installed on pump islands.
- B. Architectural Standards:
  - a. As a part of the conditional use permit application, a color illustration of all building elevations shall be submitted.
  - b. The architectural appearance, scale, and functional plan of the building(s) and canopy shall be complementary and compatible with each other and the existing buildings in the neighborhood setting.
  - c. Exterior wall treatments such as brick, stone (natural or artificial), decorative concrete block and stucco shall be used.
- C. Canopy: A protective canopy structure may be located over the pump island(s) as an accessory structure. The canopy shall meet the following performance standards:
  - a. The edge of the canopy shall be thirty feet (30') or more from the front and/or side lot line; provided, that adequate visibility both on site and off site is maintained.
  - b. The canopy shall not exceed eighteen feet (18') in height and shall provide fourteen feet (14') of clearance to accommodate a semitrailer truck passing underneath.
  - c. The canopy fascia shall not exceed three feet (3') in vertical height.
  - d. The architectural design, colors, and character of the canopy shall be consistent with the principal building on the site.
  - e. Signage may be allowed on a detached canopy in lieu of wall signage on the principal structure; provided, that the individual canopy sign does not exceed more than twenty percent (20%) of the canopy facade facing a public right of way.
  - f. Canopy posts/signposts shall not obstruct traffic or the safe operation of the gas pumps.
- D. Pump Islands: Pump islands shall comply with the following performance standards:
  - a. Pump islands shall be elevated six inches (6") above the traveled surface of the site.
  - b. All pump islands shall be set at least thirty feet (30') back from any property line. Additionally, the setback between the pump islands' curb faces shall be at least twenty four feet (24').
- E. Dust Control And Drainage: The entire site, other than that taken up by a building, structure, or plantings, shall be surfaced with asphalt, concrete, cobblestone, or paving brick. Plans for surfacing and drainage shall be subject to approval of the city engineer. Drainage from all fueling areas shall be directed to an oil/grit separator. Minimum design standards for the oil/grit separator shall include the following:
  - a. A minimum of four hundred (400) cubic feet of permanent pool storage capacity per acre of drainage area.
  - b. A minimum pool depth of four feet (4').
  - c. A minimum oil containment capacity of eight hundred (800) gallons.

- d. A minimum maintenance/inspection of two (2) times per year and/or after measurable spill events. A "measurable spill" shall be defined by the Minnesota pollution control agency (MPCA). Any measurable spill event shall be reported to the MPCA.

F. Landscaping:

- a. Minimum required green area shall be in accordance with the provisions of chapter 10 of this title.
- b. At the boundaries of the lot, the following landscape area shall be required:
  - i. From side and rear property lines, an area of not less than ten feet (10') wide shall be landscaped in compliance with chapter 10 of this title.
  - ii. From all road rights of way, an area of not less than fifteen feet (15') wide shall be landscaped in compliance with chapter 10 of this title.
  - iii. Where lots abut residentially zoned property, a buffer yard of not less than twenty feet (20') wide shall be landscaped and screened in accordance with chapter 10 of this title.
  - iv. The property owner shall be responsible for maintenance of all landscaping, including within the boulevard.

G. Exterior Lighting: The lighting shall be accomplished in such a way as to have no direct source of light visible from adjacent residential uses or from the public right of way and shall be in compliance with section 11-4-7 of this title. A comprehensive lighting plan shall be submitted as part of the conditional use permit application and shall be subject to the following performance standards:

- a. Canopy Lighting: Canopy lighting under the canopy structure shall consist of canister spotlights recessed into the canopy. No portion of the light source or fixture may extend below the ceiling of the canopy. Total canopy illumination below the canopy may not exceed one hundred fifteen (115) foot-candles at ground level.
- b. Illumination: Maximum site illumination shall not exceed the limits set forth in section 11-4-7 of this title.
- c. Access: Vehicular access points shall create a minimum of conflict with through traffic movement and shall comply with chapter 9 of this title.

H. Circulation And Loading: The site design shall accommodate adequate turning radius and vertical clearance for a semitrailer truck. Designated loading areas shall be exclusive of off street parking stalls and drive aisles. A site plan shall be provided to illustrate adequate turning radius, using appropriate engineering templates.

I. Parking:

- a. Parking spaces shall be calculated solely upon the use(s) and the square footage of the principal building(s).
- b. Parking spaces shall be screened from abutting residential properties in accordance with chapter 9 of this title.

J. Noise: The public address system shall not be audible at any property line. Playing of music or advertisement from the public address system is prohibited. Noise control shall be required as regulated in section 11-4-8 of this title.

K. Outside Storage, Sales And Service: No outside storage or sales shall be allowed, except as follows:

- a. Public phones may be located on site as long as they do not interrupt on site traffic circulation and are not located in a yard abutting residentially zoned property.
  - b. Propane sales of twenty (20) pound capacity tanks may be located outside, provided the propane tanks are secured in a locker and meet all state and city building and fire codes.
  - c. A compressed air service area may be located on site as long as it does not interrupt on site traffic circulation.
- L. Litter Control: The operation shall be responsible for litter control on the subject property, which is to occur on a daily basis. Trash receptacles shall be provided at a convenient location on site to facilitate litter control.
- M. Bottled Gas Sales: Bottled gas sales shall be allowed as an accessory use, provided:
- a. All flammable materials, including liquids and rags, shall conform with the applicable provisions of the fire code adopted in section 10-2-1 of this code.
  - b. Sales activities shall be reviewed and subject to conditions set forth by the city fire marshal.
  - c. Equipment related to the sale of bottled gas shall be secured inside the principal or an accessory building.
- N. Additional Stipulations: All conditions pertaining to a specific site are subject to change when the city council, upon investigation in relation to a formal request, finds that the general welfare and public betterment can be served as well or better by modifying or expanding the conditions set forth herein.



## Store Engineering

PHONE 608-793-5555

FAX 608-781-8960

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1626 Oak St., P.O. Box 2107

La Crosse, WI 54602

[www.kwiktrip.com](http://www.kwiktrip.com)

June 10, 2020

City of Elko New Market  
Renee Christianson – Community Development Specialist  
601 Main Street  
Elko New Market, MN 55054

**RE: Kwik Trip Store 1116 at SW Corner of Co Rd 2 and France Avenue**

Dear Ms. Christianson:

This letter is intended to accompany the submittal for our application to the City of Elko for the requested Rezone, Variance, Preliminary Plat, Final Plat and Conditional Use Permit. Please accept this letter as our request to be placed on the next available Planning Commission Meeting Agenda as well as the City Council Meeting Agenda.

Kwik Trip, Inc. is proposing the construction of a 10,950 square foot Convenience store with an integral Single Bay Carwash and a 10-dispenser main fueling canopy as well as a two-lane side diesel canopy. This site will be accessed from a right-in only as well as a full access movement from France Avenue. There will also be a shared access road agreement with the adjacent property creating ingress/egress to the site from Market Place Blvd. Included with the applications in the submittal are 1 full size 24x36 and 1 -11x17 copies of the Site Improvement Plans. I have also included the ALTA Survey, Preliminary Plat, Final Plat, Building and Canopy Elevations, Signage Plans and the Storm Water Management Plan.

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**OUR MISSION**

*To serve our customers and community more effectively than anyone else by treating our customers, co-workers and suppliers as we, personally, would like to be treated, and to make a difference in someone's life.*

---

### Operations

The requested hours of operation will be 24 hours for all uses. The type of products that will be sold will be similar to that of our existing stores throughout the mid-west: fresh produce, bakery and dairy, hot and cold food and beverages, fresh meat and groceries, tobacco products, lotto, convenience store merchandise, alcohol, gasoline, diesel, E-85, ice and propane. The outside merchandising of products is being requested next to the store (two ice chests and one propane cage) and underneath the proposed main canopy. To ensure that the freshest products are sold in our stores, we request that daily deliveries be allowed.

### Buildings, Architecture and Site Design

The architectural elements in this state-of-the-art building consist of a full brick cladding, standing seam metal roof, store front aluminum openings and stucco accents. Extensive landscaping, modern storm water facilities, monument and wall signage, customer and employee parking, concrete paving with curb and gutter are also included in the overall site design.

### Investment in the City

This project will be a multi-million-dollar investment in the City of Elko New Market. Not only in the physical improvements and development of a vacant parcel, but also an investment of approximately 25 to 30 new permanent jobs in the City. The projected payroll here is estimated to be nearly \$500,000 annually.

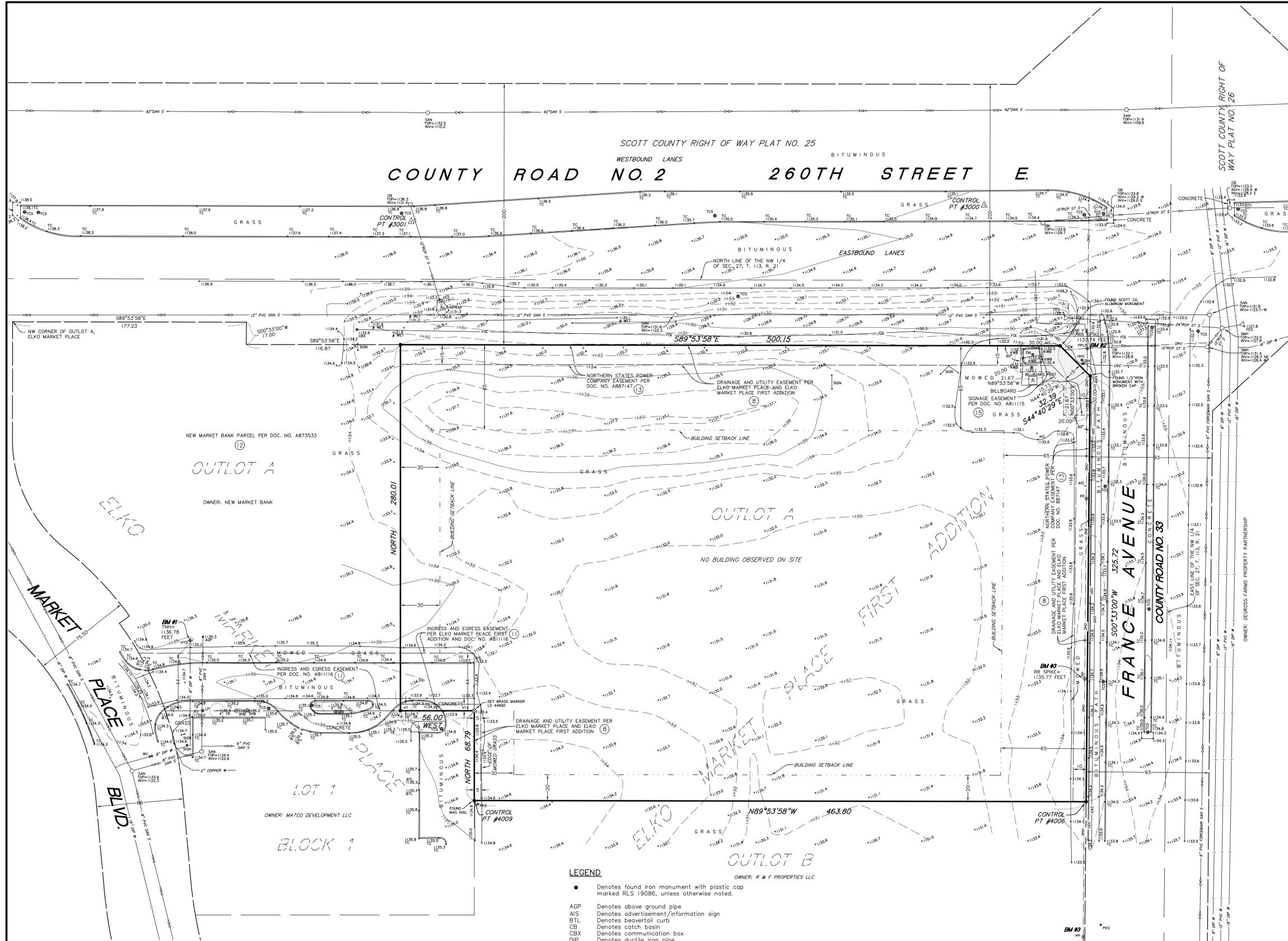
### Community Partner

We pride ourselves in being an asset in the communities where we are located. Families can walk or ride their bikes to our stores. Retirees on fixed income can access fresh groceries like milk, eggs, bread and fruit just steps from their car. We take pride in giving back to the communities we serve with charitable donations and by partnering with local non-profits. Kwik Trip would be happy to provide any additional information or answer any questions or concerns the City of Elko New Market may have with our submittal. Please feel free to call or email with any questions you may have.

Sincerely,

Steven Lowe  
Development/Construction Manager  
Store Engineering  
slowe@kwiktrip.com  
608-793-5954





**DESCRIPTION OF PROPERTY SURVEYED**  
 (Per First American Title Insurance Company dba Tri-County Abstract and Title Guaranty Commitment for Title Insurance Commitment No. 1528140, commitment date August 27, 2019.)  
 Outlot A, Elko Market Place First Addition, Scott County, Minnesota.  
 NOTE: This property is Abstract.

**PLAT RECORDING INFORMATION**  
 The plot of ELKO MARKET PLACE FIRST ADDITION was filed of record on December 11, 2011 as Document No. AB95126.  
 [ ] Bearings and/or dimensions listed within brackets are per plat or record documents.

**TITLE COMMITMENT**  
 First American Title Insurance Company dba Tri-County Abstract and Title Guaranty Commitment for Title Insurance Commitment No. 1528140, commitment date August 27, 2019, was relied upon as to matters of record.

**Schedule B Exceptions:**  
 1) Exceptions are indicated on survey with circled numbers unless otherwise noted. Items not listed below are standard exceptions and/or are not survey related.  
 8) An easement shown or dedicated on the Map as referred to in the legal description.  
 For: Utility and Drainage Purposes [shown on survey].  
 9) Terms and conditions of Agreement dated December 29, 2011, Filed January 6, 2012 as Document No. 895752. [not survey related].  
 10) Subject to Planned Unit Development Agreement and Development Contract dated March 17, 2006, filed May 19, 2008, as Document No. A73844, between the City of Elko and R&F Properties, LLC. [not survey related].  
 Amended by Amendment to Planned Unit Development Agreement and Development Contract dated October 23, 2008, filed October 28, 2008, as Document No. AB1112.  
 Amended by Amendment to Planned Unit Development Agreement Development Contract dated January 13, 2011, filed February 11, 2011, as Document No. AB74758.  
 Amendment dated April 10, 2014, filed April 21, 2014, as Document No. 956842.  
 Amendment dated April 9, 2015, filed July 7, 2015, as Document No. 983002.  
 Amendment dated October 22, 2015, filed November 20, 2015, as Document No. 990992.  
 Amendment dated December 1, 2016, filed December 22, 2016, as Document No. 101717.  
 11.) Subject to Easement for Ingress and egress purposes as created in Declaration of Easement dated October 27, 2008, filed October 28, 2008, as Document No. AB1116. [shown on survey].  
 12.) Subject to non-exclusive cross easements as created in Cross Parking and Access Easement dated October 27, 2008, filed October 28, 2008, as Document No. AB1120. [Easement area is not defined as the particular development of the property is unknown, and may vary substantially from the Exhibit A depiction attached to Doc. No. 81120.]  
 As Amended by Amendment to Cross Parking and Access Easement dated January 4, 2011, Filed January 26, 2011, as Doc. No. AB73533. [The "New Market Bank" parcel is shown for reference, Exhibit "A" which describes the "Third Party" parcel was not attached to the Document provided to us.]  
 13.) Subject to an Easement granted to Minnesota Utilities Co. as created in Easement Indenture dated October 30, 1942, filed August 29, 1944 in Book 104 of Deeds on pages 202-3. [Easement is blanket in nature].  
 As confined to the utility and roadway easements as shown on the plat of Elko Market Place as reserved in Partial Release of Easement dated August 29, 2011, filed September 9, 2011, as Doc. No. AB87147. [shown on survey].  
 14.) Declaration and Restrictive Covenants for Elko Market Place dated December 30, 2010, filed January 28, 2011, as Doc. No. AB73534. [Easements are blanket in nature].  
 15.) Subject to Easement for signage purposes over the Northeastern corner of subject premises as created in Declaration of Easements dated October 27, 2008, filed October 28, 2008, as Document No. AB1115. [shown on survey].

**GENERAL NOTES**

- 1.) Survey coordinate basis: Scott County Coordinate System.
- 2.) "No Buildings Observed" on the surveyed property in the process of conducting the fieldwork.
- 3.) There are proposed changes in street right of way lines per the City of Elko New Market Planning Department. They will be addressed during the replating process. There was no evidence of recent street or sidewalk construction or repairs observed in the process of conducting the fieldwork.
- 4.) Wetland shown hereon is per delineation markers observed in the process of conducting the fieldwork.
- 5.) Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 21 of Table A, calls for sanitary sewers, storm sewer inverts, adjacent structures, sanitary, storm and water pipe sizes, street widths, street medians, adjacent and cross driveways to be shown.
- 6.) Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 22 of Table A, calls for 4 control points to be set and note the northing, easting and elevation of the points.

**UTILITY NOTES**

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call locates requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.
- 2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket number 192612731.
- 4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

**FLOOD ZONE NOTE**

- 1.) The subject property appears to lie within Zone X (Areas determined to be outside the 0.2% annual chance floodplain) per the National Flood Insurance Program, Flood Insurance Rate Map Community Flood No. 270643C0125C, dated February 19, 1987. The City of Elko New Market, Scott County, has annexed into Scott County FEMA panel 2704280125C, dated February 19, 1987. This information was obtained from the FEMA Map Service Center web site.

**ZONING NOTES**

- 1.) Zoning information and building setback depiction shown hereon is per zoning letter dated August 30, 2019 prepared by City of Elko New Market, Community Development Department.

Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 6(c) of Table A specifies "current zoning classification, setback requirements, the height and floor space area restrictions, and parking requirements". Only those items set forth in said letter have been listed.

The property is currently zoned Planned Unit Development (PUD) and is subject to requirements of a Planned Unit Development Agreement and subsequent amendments (attached). The PUD Agreement states that the property is subject to the B5 zoning district requirements, except as otherwise stated in the PUD Agreement. Any future use within this commercial area requires an amendment to the PUD for the proposed use. Another option would be to rezone the property to a specific zoning district to remove it from the PUD.

Motor fuel stations and commercial car washes are listed as Conditional Use in the B5 district, performance standards relating to each are contained within the chapter. All other applicable sections of the Zoning Ordinance also apply. Below is a summary of miscellaneous requirements as required:

**Building setback requirements are as follows:**

- o From CS&H 2 - 65' from right-of-way line
- o From France Avenue - 65' from right-of-way line
- o Side Setback - 20' - from south property line
- o Rear Setback - 30' - from west property line

**Maximum building height - 35'**

No maximum floor space or impervious surface requirements, other than those dictated by the landscaping requirements.

**Parking requirements**

- o Convenience store - 1 space per 100 square feet of floor area

Zoning letter provided recites zoning code. Due to space limitations, said zoning letter is included herewith for reference.

**PARKING**  
None observed

**AREA**  
177,724 square feet or 4.080 acres

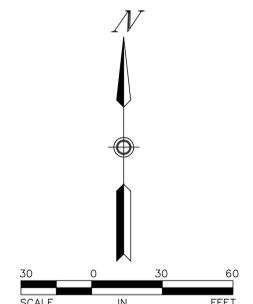
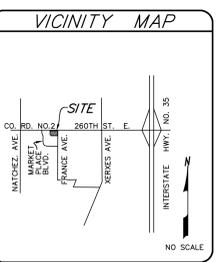
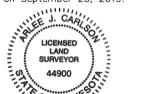
**SURVEYOR'S CERTIFICATION**

To Kwik Trip, Inc., a Wisconsin Corporation, First American Title Insurance Company dba Tri-County Abstract and Title Guaranty.

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 6(a), 6(b), 7(a), 8, 9, 11, 14, 17, 18, 19, 21 and 22 of Table A thereof. The fieldwork was completed on September 25, 2019.

Dated this 4th day of October, 2019.

SUNDE LAND SURVEYING, LLC.  
 By: *Arlee J. Carlson*  
 Arlee J. Carlson, P.L.S. Minn. Lic. No. 44900



- LEGEND**
- Denotes found iron monument with plastic cap marked RLS 19086, unless otherwise noted.
  - AGP Denotes above ground pipe
  - AIS Denotes advertisement/information sign
  - BT Denotes beavertail curb
  - CB Denotes catch basin
  - CBX Denotes communication box
  - DIP Denotes ductile iron pipe
  - EM Denotes electric meter
  - FES Denotes fire and section
  - FOL Denotes fiber optic line
  - GAS V Denotes gas valve
  - GW Denotes guy wire
  - HHC Denotes communication hand hole
  - HYD Denotes fire hydrant
  - INV Denotes structure invert
  - KWB Denotes keystone wall base
  - LP Denotes light pole
  - MB Denotes mailbox
  - OHU Denotes overhead utility lines
  - PKS Denotes parking sign
  - PP Denotes power pole
  - PPT Denotes power pole with transformer
  - PVC Denotes polyvinylchloride pipe
  - RCP Denotes reinforced concrete pipe
  - SAN Denotes sanitary manhole
  - SAN S Denotes sanitary sewer
  - SMH Denotes storm manhole
  - STS Denotes storm sewer
  - TC Denotes top of concrete curb
  - TCS Denotes traffic control sign
  - UGC Denotes underground communication line
  - W Denotes water line
  - WV Denotes water valve
  - SHB Denotes shrub
  - TR Denotes deciduous tree

- BENCHMARKS (BM)**  
(NAVD 88)
- 1.) Top of top nut of fire hydrant on east side of Market Place Blvd. northwest of post office  
Elevation = 1136.78 feet
  - 2.) Top of railroad spike in face of 1st power pole south of County Road No. 2 on west side of France Avenue.  
Elevation = 1133.74 feet
  - 3.) Top of railroad spike in face of 3rd power pole south of County Road No. 2 on west side of France Avenue.  
Elevation = 1135.77 feet

**CONTROL STATIONS**

POINT	NORTHING	EASTING	ELEV.
3000	136889.249	496712.727	1135.22
3001	136876.829	496278.346	1137.49
4006	136432.857	496789.823	1134.29
4009	136433.746	496326.089	1134.86

- LIST OF POSSIBLE ENCROACHMENTS**
- The following list of possible encroachments is only the opinion of this surveyor; should not be interpreted as a legal opinion and should not be interpreted as a complete listing.
- A) Possible encroachments are indicated on survey with boxed letters as listed below.
- A.) Billboard outside of easement

Added note to Sch. B2 - Item 13 [MD 10-23-2019]

Revision [ ] By [ ] Date [ ]

Drawing Title:  
**ALTA/NSPS LAND TITLE SURVEY FOR:  
 KWIK TRIP, INC.  
 STORE # XXX  
 NEW MARKET, MN**

**SUNDE LAND SURVEYING**  
 www.sundes.com

9001 East Bloomington Freeway (25W) • Suite 118  
 Bloomington, Minnesota 55420-3435  
 952-881-2455 (Fax: 952-888-9526)

Project: 2019-136 By/Pg: 1177/01 Date: 10/04/2019  
 Township: 13 Range: 21 Section: 27  
 File: 2019136001.dwg Sheet: 1 of 1

DESCRIPTION OF PROPERTY SURVEYED  
(Per First American Title Insurance Company for Title Insurance File No. 1528140, Revision No. 2, commitment date March 24, 2020.)

Outlot A, Elko Market Place First Addition, Scott County, Minnesota.  
NOTE: This property is Abstract.

PLAT RECORDING INFORMATION  
The plot of ELKO MARKET PLACE FIRST ADDITION was filed of record on December 11, 2011 as Document No. AB85126.

TITLE COMMITMENT  
First American Title Insurance Company for Title Insurance File No. 1528140, Revision No. 2, commitment date March 24, 2020, was relied upon as to matters of record.

Schedule B Exceptions:  
① Exceptions are indicated on survey with circled numbers unless otherwise noted. Items not listed below are standard exceptions and/or are not survey related.

8.) An easement shown or dedicated on the Map as referred to in the legal description  
For: Utility and Drainage Purposes [shown on survey]

9.) Terms and conditions of Agreement dated December 29, 2011, filed January 6, 2012 as Document No. 895752. [Not survey related]

10.) Subject to Planned Unit Development Agreement and Development Contract dated March 17, 2006, filed May 19, 2006, as Document No. A739444, between the City of Elko and R&F Properties, LLC. [Not survey related]

Amended by Amendment to Planned Unit Development Agreement and Development Contract dated October 23, 2008, filed October 28, 2008, as Document No. AB1112.

Amended by Amendment to Planned Unit Development Agreement Development Contract dated January 13, 2011, filed February 11, 2011, as Document No. AB74758.

Amendment dated April 10, 2014, filed April 21, 2014, as Document No. 956842.

Amendment dated October 22, 2015, filed November 5, 2015, as Document No. 990992.

Amendment dated December 1, 2016, filed December 22, 2016, as Document No. 101717.

Amendment dated October 10, 2019, filed December 9, 2019, as Document No. A108038.

11.) Subject to Easement for Ingress and egress purposes as created in Declaration of Easement dated October 27, 2008, filed October 28, 2008, as Document No. AB1116. [shown on survey]

12.) Subject to non-exclusive cross easements as created in Cross Parking and Access Easement dated October 27, 2008, filed October 28, 2008, as Document No. AB1120. [Easement area is not defined as the particular development of the property is unknown, and may vary substantially from the Exhibit A depiction attached to Doc. No. AB1120.]

As amended by Amendment to Cross Parking and Access Easement dated January 4, 2011, filed January 26, 2011, as Doc. No. AB73533. [The "New Market Bank" parcel is shown for reference, Exhibit "A" which describes the "Third Party" parcel is not attached to the Document provided to us.]

13.) Subject to an Easement granted to Minnesota Utilities Co. as created in Easement Indenture dated October 30, 1942, filed August 29, 1944 in Book 14 of Deeds on pages 202-3. [Easement is blanket in nature]

As confined to the utility and roadway easements as shown on the plot of Elko Market Place as reserved in Partial Release of Easement dated August 29, 2011, filed September 9, 2011, as Doc. No. AB8747.

14.) Declaration and Restrictive Covenants for Elko Market Place dated December 30, 2010, filed January 28, 2011, as Doc. No. AB73534. [Easements are blanket in nature]

15.) Subject to Easement for signage purposes over the Northeast corner of subject premises as created in Declaration of Easements dated October 27, 2008, filed October 28, 2008, as Document No. AB1115. [shown on survey]

GENERAL NOTES  
1.) Survey coordinate basis: Scott County Coordinate System.

2.) "No Buildings Observed" on the surveyed property in the process of conducting the fieldwork.

3.) There are proposed changes in street right of way lines per the City of Elko Market Planning Department. They will be addressed during the replating process. There was no evidence of recent street or sidewalk construction or repairs observed in the process of conducting the fieldwork.

4.) Wetland shown hereon is per delineation markers observed in the process of conducting the fieldwork.

5.) Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 21 of Table A, calls for sanitary inverts, storm sewer inverts, adjacent structures, sanitary, storm and water pipe sizes, street widths, street medians, adjacent and cross driveways to be shown.

6.) Proposed features shown are based on a CAD drawing site plan received on 6/05/2020 and provided to us from Kwik Trip Store Engineering.

UTILITY NOTES  
1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground, as shown hereon. However, locking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call locate requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.

2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.

3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket number 192612731.

4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

FLOOD ZONE NOTE  
1.) The subject property appears to lie within Zone X (Areas determined to be outside the 0.2% annual chance floodplain) per the National Flood Insurance Program, Flood Insurance Rate Map Community Panel No. 270428025C, dated February 18, 1987. The City of Elko Market, Scott County, has annexed into Scott County FRM panel No. 270428025C, dated February 18, 1987. This information was obtained from the FEMA Map Service Center web site.

ZONING NOTES  
1.) Zoning information and building setback depiction shown hereon is per zoning letter dated August 30, 2019 prepared by City of Elko Market, Community Development Department.

Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 6(a) of Table A specifies "current zoning classification, setback requirements, the height and floor space area restrictions, and parking requirements". Only those items set forth in said letter have been listed.

The property is currently zoned Planned Unit Development (PUD) and is subject to requirements of a Planned Unit Development Agreement and subsequent amendments (attached). The PUD Agreement states that the property is subject to the B5 zoning district requirements, except as otherwise stated in the PUD Agreement. Any future use within this commercial area requires an amendment to the PUD for the proposed use. Another option would be to rezone the property to a specific zoning district to remove it from the PUD.

Motor fuel stations and commercial car washes are listed as a Conditional Use in the B5 district, performance standards relating to each are contained within the chapter. All other applicable sections of the Zoning Ordinance also apply. Below is a summary of miscellaneous requirements:

Building setback requirements are as follows:  
o From CSAH 2 - 65' from right-of-way line  
o From France Avenue - 65' from right-of-way line  
o Side Setback - 20' - from south property line  
o Rear Setback - 30' - from west property line

Maximum building height - 35'  
No maximum floor space or impervious surface requirements, other than those dictated by the landscaping requirements.

Parking requirements  
o Convenience store - 1 space per 100 square feet of floor area

Zoning letter provided recites zoning code. Due to space limitations, said zoning letter is included herewith for reference.

PROPOSED ZONING: B5  
AREA  
Lot 1 = 177,724 square feet or 4.080 acres

OWNER  
Northfield Hospital  
P.O. Box 2107  
1626 Oak Street  
Blaine, MN 55449  
Contact person: Steve Underdahl  
Phone: (507)-646-1000

DEVELOPER:  
Kwik Trip, Inc.  
P.O. Box 2107  
1626 Oak Street  
Blaine, MN 55449  
Contact person: Brad Fry  
Phone: (608)-781-8988

ARCHITECT/DESIGNER:  
Vantage Architects, Inc.  
750 N Third St Ste F,  
La Crosse, WI 54601  
Contact person: Jerrel Schomberg, AIA  
Phone: (608)-784-2729

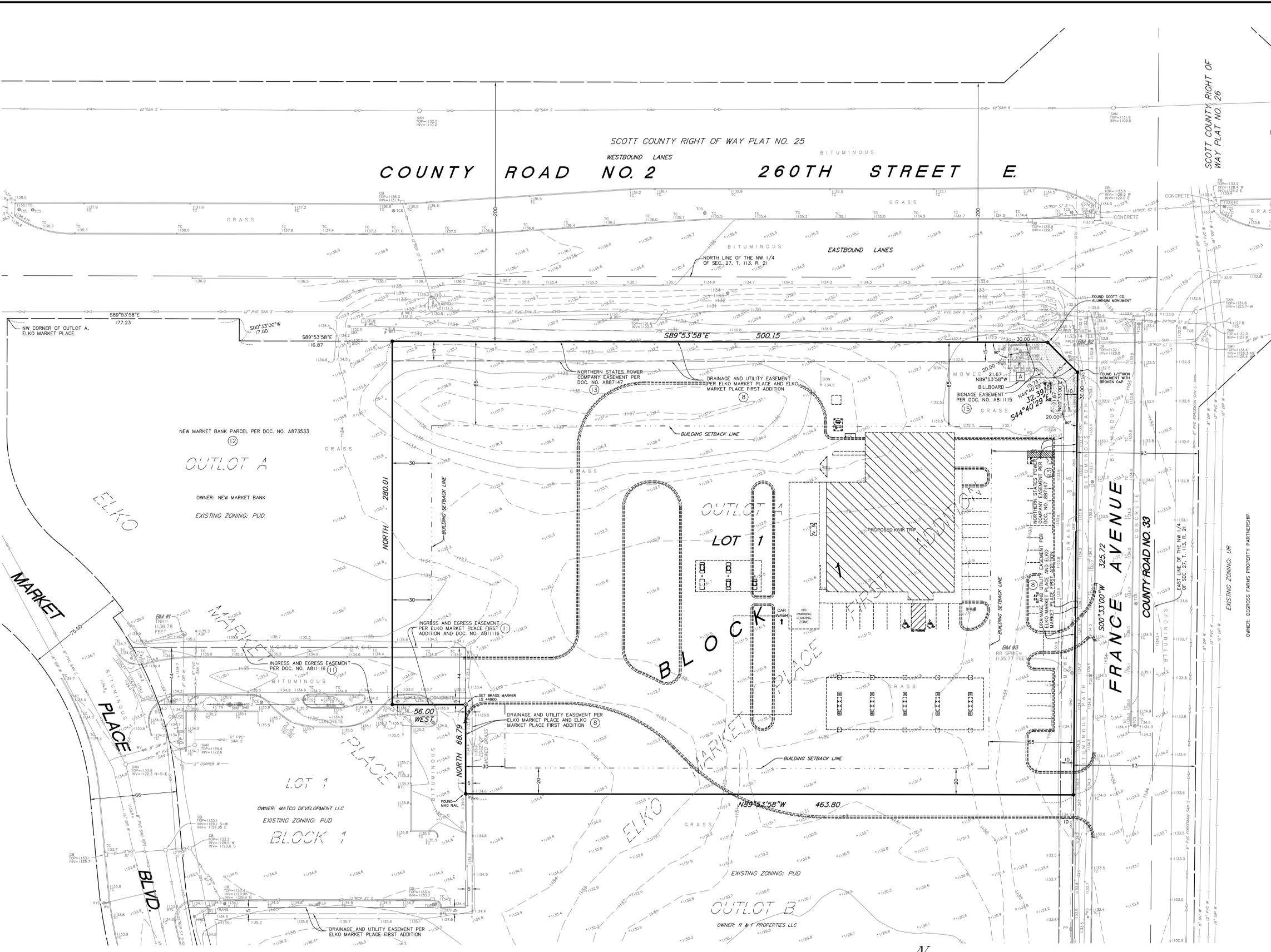
CIVIL ENGINEER:  
Carlson McCain, Inc.  
3890 Pleasant Ridge Drive NE  
Suite 100  
Blaine, MN 55449  
Contact person: Joseph T. Radach, PE  
Phone: (763)-489-7912

SURVEYOR:  
Sunde Land Surveying, LLC  
9001 E. Bloomington Freeway  
Suite 118  
Bloomington, MN 55420  
Contact person: Arlee Carlson  
Phone: (952)-881-2455

I hereby certify that this sketch, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 8th day of June, 2020  
SUNDE LAND SURVEYING, LLC  
By: Arlee J. Carlson, P.L.S., Minn. Lic. No. 44900

Project: 2019-136 Bl/Pg: 1177/01 Date: 06/09/2020  
Township: 113 Range: 21 Section: 27 Sheet: 1 of 1  
File: 2019136001plat.dwg



**BENCHMARKS (BM)**  
(NAVD 85)

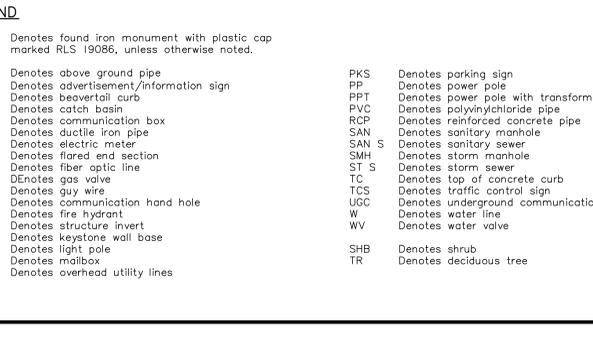
1.) Top of nut top of fire hydrant on east side of Market Place Blvd. northwest of post office.  
Elevation = 1136.78 feet

2.) Top of railroad spike in face of 1st power pole south of County Road No. 2 on west side of France Avenue.  
Elevation = 1133.74 feet

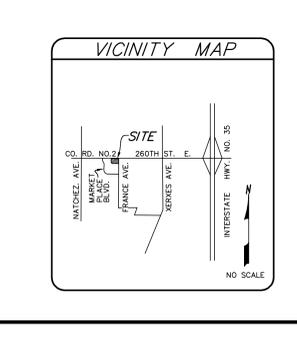
3.) Top of railroad spike in face of 3rd power pole south of County Road No. 2 on west side of France Avenue.  
Elevation = 1135.77 feet

**LEGEND**

●	Denotes found iron monument with plastic cap marked RLS 19086, unless otherwise noted.	PKS	Denotes parking sign
AGP	Denotes above ground pipe	PP	Denotes power pole
AIS	Denotes advertisement/information sign	PPT	Denotes power pole with transformer
BT	Denotes beaver tail curb	PVC	Denotes polyvinylchloride pipe
CB	Denotes catch basin	RCP	Denotes reinforced concrete pipe
CBX	Denotes communication box	SAN	Denotes sanitary manhole
DIP	Denotes ductile iron pipe	SAN S	Denotes sanitary sewer
EM	Denotes electric meter	SMH	Denotes storm manhole
FES	Denotes flared end section	ST S	Denotes storm sewer
FOL	Denotes fiber optic line	TC	Denotes top of concrete curb
GAS V	Denotes gas valve	TCS	Denotes traffic control sign
GW	Denotes guy wire	UGC	Denotes underground communication line
HHC	Denotes communication hand hole	W	Denotes water line
HYD	Denotes fire hydrant	WV	Denotes water valve
INV	Denotes structure invert	SHB	Denotes shrub
KWB	Denotes keystone wall base	TR	Denotes deciduous tree
LP	Denotes light pole		
MB	Denotes mailbox		
OHU	Denotes overhead utility lines		



Scale: 0 30 60 FEET  
IN



SUNDE LAND SURVEYING, LLC  
9001 East Bloomington Freeway (25W) • Suite 118  
Bloomington, Minnesota 55420-3435  
952-881-2455 (Fax: 952-888-9526)  
www.sundes.com

Project: 2019-136 Bl/Pg: 1177/01 Date: 06/09/2020  
Township: 113 Range: 21 Section: 27 Sheet: 1 of 1  
File: 2019136001plat.dwg

# KWIK TRIP 1116

SCOTT COUNTY RIGHT OF WAY PLAT NO. 25

(260TH STREET E.)

COUNTY ROAD NO. 2

SCOTT COUNTY RIGHT OF WAY PLAT NO. 26

KNOW ALL PERSONS BY THESE PRESENTS: That Northfield Hospital, an instrumentality of the City of Northfield, a Minnesota Municipal Corporation, owner of the following described property situated in the County of Scott, State of Minnesota, to wit:

Outlot A, ELKO MARKET PLACE FIRST ADDITION, Scott County, Minnesota.

Has caused the same to be surveyed and platted as KWIK TRIP 1116 and does hereby donate and dedicate to the public for public use forever the drainage and utility easements as shown on this plat.

In witness whereof said Northfield Hospital, an instrumentality of the City of Northfield, a Minnesota Municipal Corporation, has caused these presents to be signed by its proper officer this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

SIGNED: \_\_\_\_\_ its \_\_\_\_\_

STATE OF \_\_\_\_\_

COUNTY OF \_\_\_\_\_

The instrument was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, by \_\_\_\_\_, of Northfield Hospital, an instrumentality of the City of Northfield, a Minnesota Municipal Corporation, on behalf of the corporation.

Notary Public, \_\_\_\_\_  
My Commission Expires: \_\_\_\_\_

I, Arlee J. Carlson, do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been, or will be correctly set within one year; that all water boundaries and wet lands as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on this plat; and all public ways are shown and labeled on this plat.

Arlee J. Carlson, Licensed Land Surveyor  
Minnesota License No. 44900

The foregoing Surveyor's Certificate was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, by Arlee J. Carlson, Licensed Land Surveyor.

Notary Public, \_\_\_\_\_ County, \_\_\_\_\_  
My Commission Expires: \_\_\_\_\_

CITY COUNCIL, CITY OF ELKO NEW MARKET, MINNESOTA

This plat was approved by the City Council of the City of Elko New Market, Minnesota, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, and hereby certifies compliance with all requirements as set forth in Minnesota Statutes, Section 505.03, Subdivision 2.

By: \_\_\_\_\_ Mayor  
Clerk

CITY ATTORNEY, CITY OF ELKO NEW MARKET, MINNESOTA

I hereby certify that I have examined this plat of \_\_\_\_\_ and do hereby recommend this plat for approval as to form this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

City Attorney

PLANNING COMMISSION, CITY OF ELKO NEW MARKET, MINNESOTA

Approved by the Planning Commission of the City of \_\_\_\_\_, Minnesota, at a meeting thereof, on the \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

Chairperson and/or Clerk

SCOTT COUNTY SURVEYOR:

Pursuant to Minnesota Statutes, Chapter 389.09, Subd. 1, as amended, this plat has been reviewed and approved this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

Scott County Surveyor

SCOTT COUNTY AUDITOR/TREASURER:

I hereby certify that the current and delinquent taxes on the lands described within are paid and the transfer entered this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

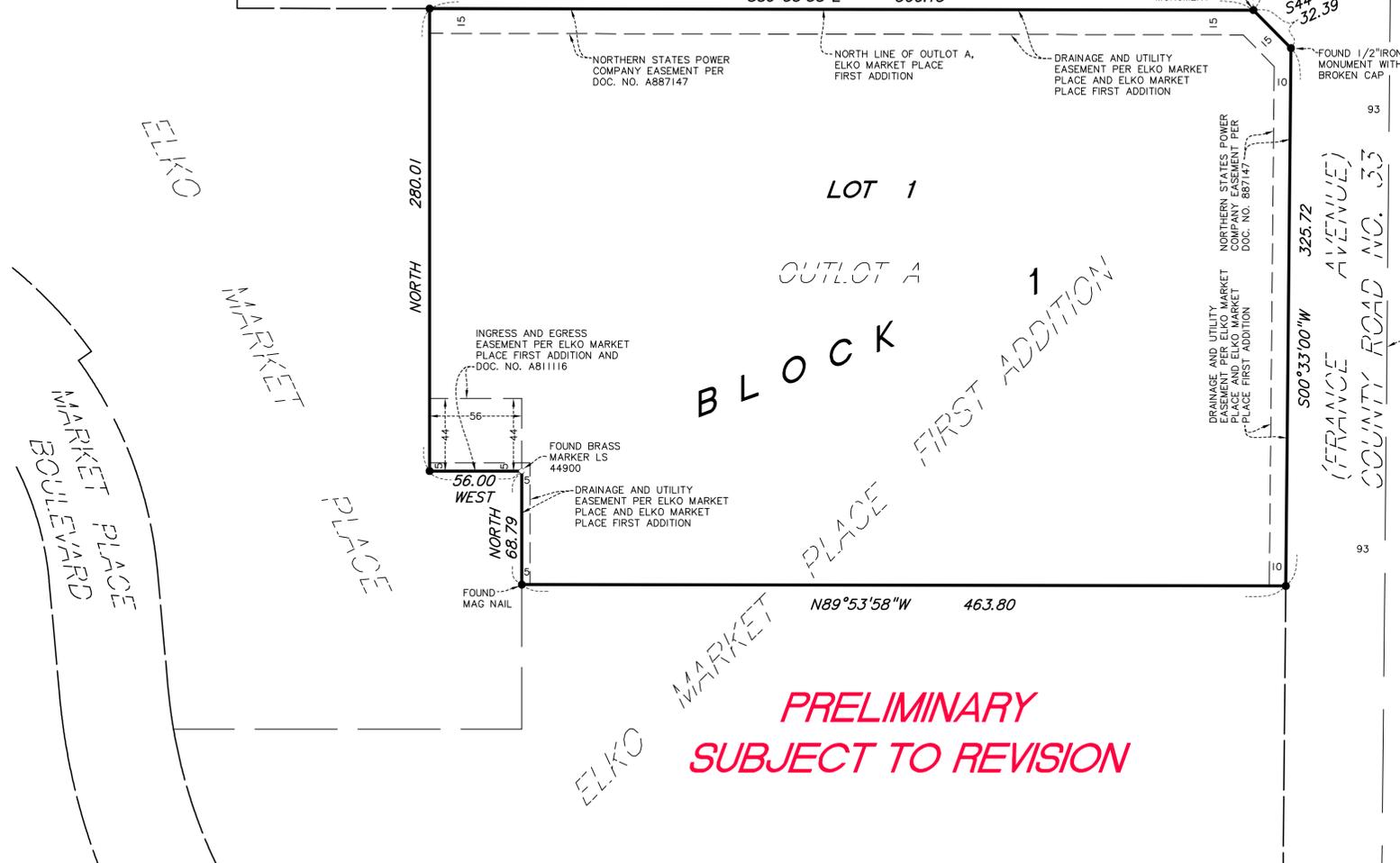
SCOTT COUNTY AUDITOR/TREASURER

By: \_\_\_\_\_ Deputy

SCOTT COUNTY RECORDER:

I hereby certify that this plat was recorded in the office of the County Recorder for record this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, at \_\_\_\_\_ o'clock, \_\_\_\_M. as Document No. \_\_\_\_\_.

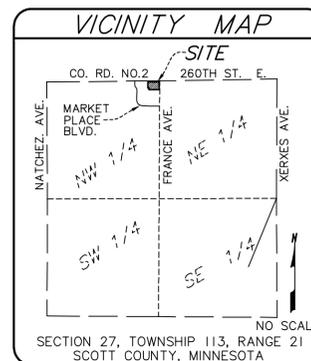
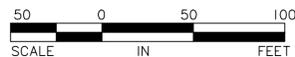
Scott County Recorder



**PRELIMINARY  
SUBJECT TO REVISION**

The north line of Outlot A, ELKO MARKET PLACE FIRST ADDITIONS is assumed to have a bearing of S89°53'58"E.

- Denotes 1/2 inch by 14 inch iron monument set and marked by License No. 44900.
- Denotes iron monument found marked by License No. 44890, unless otherwise noted.



# KWIK TRIP STORE #1116

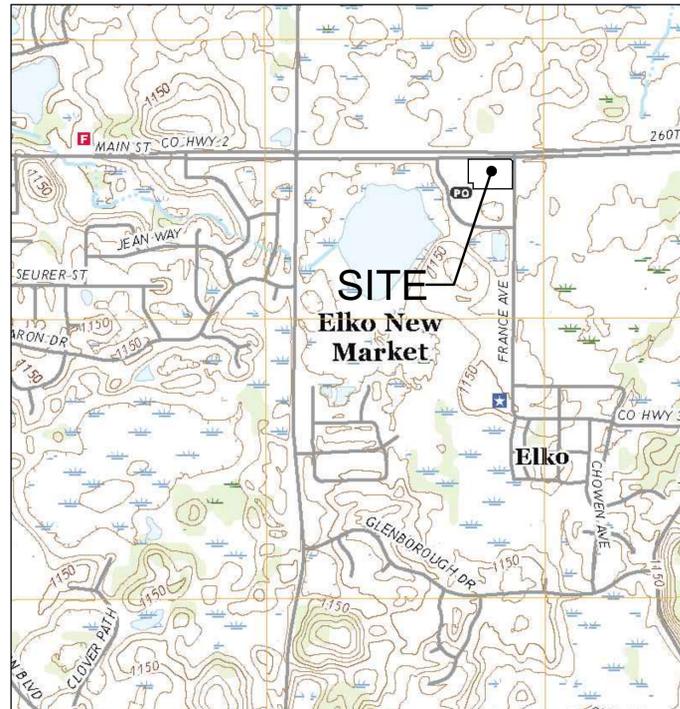
## CITY SUBMITTAL CITY OF ELKO NEW MARKET SCOTT COUNTY, MINNESOTA

**OWNER**  
KWIK TRIP, INC  
STEVEN LOWE  
1626 OAK STREET  
LA CROSSE, WI 54602  
PHONE: 608-793-5954  
EMAIL: [slowe@kwiktrip.com](mailto:slowe@kwiktrip.com)

**CIVIL ENGINEER**  
CARLSON MCCAIN, INC.  
JOSEPH RADACH  
3890 PHEASANT RIDGE DR NE, #100  
BLAINE, MN 55449  
PHONE: 763-489-7912  
EMAIL: [jradach@carlsonmccain.com](mailto:jradach@carlsonmccain.com)

**SITE DESIGNER**  
CARLSON MCCAIN, INC.  
JOSEPH RADACH  
3890 PHEASANT RIDGE DR NE, #100  
BLAINE, MN 55449  
PHONE: 763-489-7912  
EMAIL: [jradach@carlsonmccain.com](mailto:jradach@carlsonmccain.com)

**SURVEYOR**  
SUNDE LAND SURVEYING  
ARLEE CARLSON  
9001 EAST BLOOMINGTON FREEWAY  
SUITE 118  
BLOOMINGTON, MN 55420  
PHONE: 952-881-2455  
EMAIL: [arlee.carlson@sunde.com](mailto:arlee.carlson@sunde.com)



SITE LOCATION MAP



AERIAL LOCATION MAP

**SHEET INDEX**

ALTA	SURVEY
SP1	EXISTING CONDITIONS & REMOVALS PLAN
SP2	SITE PLAN (LAYOUT)
SP2.1	SITE PLAN (KEYNOTE)
SP2.2	TURNING MOVEMENTS
SP3	GRADING & EROSION CONTROL PLAN
SP4	UTILITY PLAN
SP4.1	UTILITY PLAN (STORM SEWER)
SP5	STORMWATER POLLUTION PREVENTION PLAN
SP6	DETAILS
SP7	DETAILS
SP8	DETAILS
L1	LANDSCAPE PLAN
L2	LANDSCAPE PLAN
E1	PHOTOMETRIC LIGHTING PLAN



KWIK TRIP, Inc.  
P.O. BOX 2107  
1626 OAK STREET  
LA CROSSE, WI 54602-2107  
PH. (608) 781-8988  
FAX (608) 781-8960

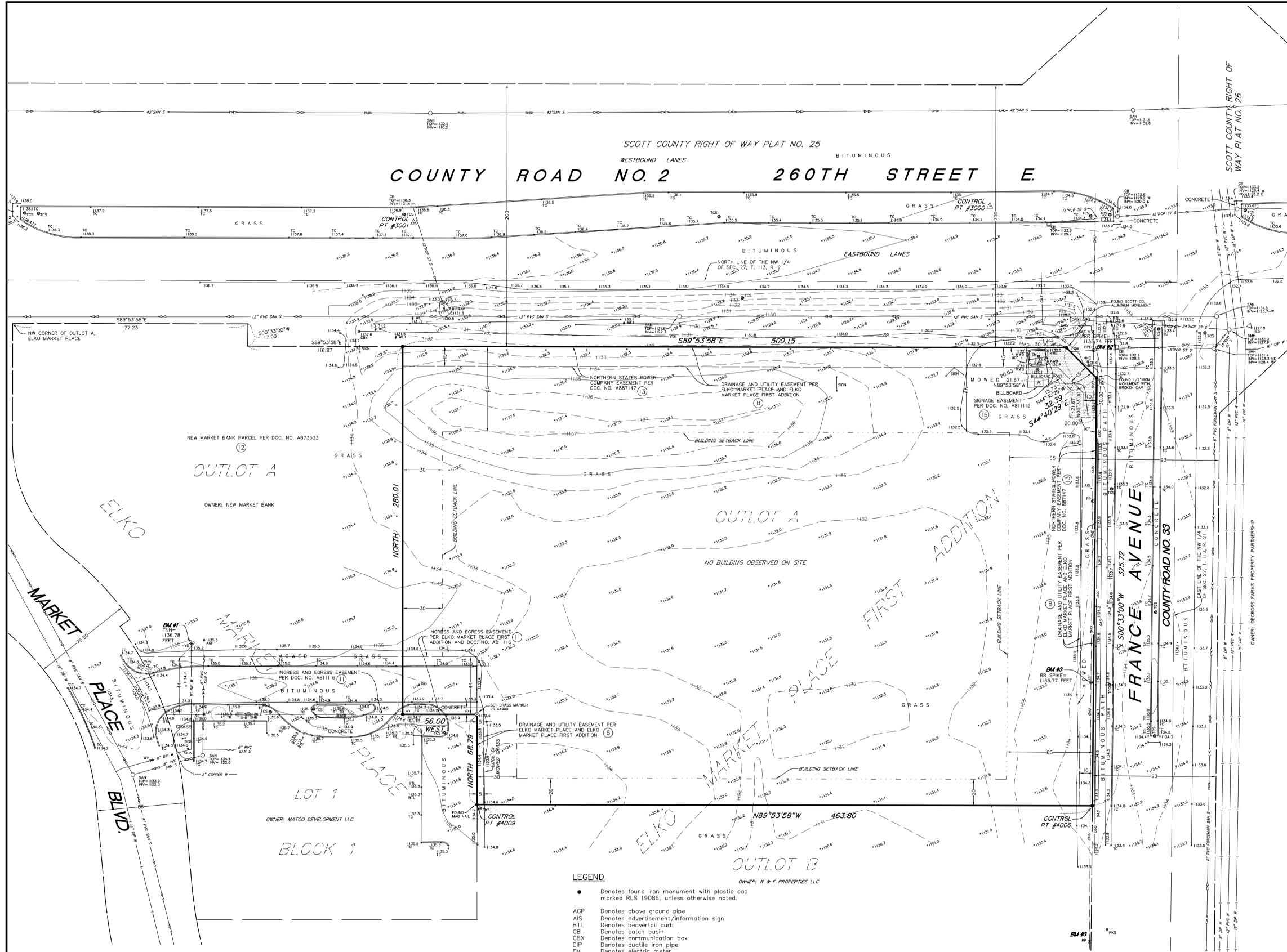


I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Name: Joseph T. Radach, P.E.  
Signature: *[Signature]*  
Date: 05/15/20 License #: 45889

**COVER SHEET**  
CONVENIENCE STORE #1116  
WITH SINGLE BAY CARWASH  
AND SIDE DIESEL  
260TH ST E & FRANCE AVE  
ELKO NEW MARKET, MINNESOTA

#	DATE	DESCRIPTION

DRAWN BY: JTR  
SCALE: GRAPHIC  
PROJ. NO.: 8622-00  
DATE: 2020-05-15  
SHEET: 1116 G000



**DESCRIPTION OF PROPERTY SURVEYED**  
 (Per First American Title Insurance Company dba Tri-County Abstract and Title Guaranty Commitment for Title Insurance Commitment No. 1528140, commitment date August 27, 2019.)  
 Outlot A, Elko Market Place First Addition, Scott County, Minnesota.  
 NOTE: This property is Abstract.

**PLAT RECORDING INFORMATION**  
 The plot of ELKO MARKET PLACE FIRST ADDITION was filed of record on December 11, 2011 as Document No. A895126.  
 [ ] Bearings and/or dimensions listed within brackets are per plat or record documents.

**TITLE COMMITMENT**  
 First American Title Insurance Company dba Tri-County Abstract and Title Guaranty Commitment for Title Insurance Commitment No. 1528140, commitment date August 27, 2019, was relied upon as to matters of record.

**Schedule B Exceptions:**  
 1) Exceptions are indicated on survey with circled numbers unless otherwise noted. Items not listed below are standard exceptions and/or are not survey related.  
 8) An easement shown or dedicated on the Map as referred to in the legal description.  
 For: Utility and Drainage Purposes [shown on survey].  
 9) Terms and conditions of Agreement dated December 29, 2011, Filed January 6, 2012 as Document No. 895752. [not survey related].  
 10) Subject to Planned Unit Development Agreement and Development Contract dated March 17, 2006, filed May 19, 2008, as Document No. A738444, between the City of Elko and R&F Properties, LLC. [not survey related].  
 Amended by Amendment to Planned Unit Development Agreement and Development Contract dated October 23, 2018, filed October 28, 2018, as Document No. A81112.  
 Amended by Amendment to Planned Unit Development Agreement Development Contract dated January 13, 2011, filed February 11, 2011, as Document No. A874758.  
 Amendment dated April 10, 2014, filed April 21, 2014, as Document No. 956842.  
 Amendment dated April 9, 2015, filed July 7, 2015, as Document No. 983002.  
 Amendment dated October 22, 2015, filed November 20, 2015, as Document No. 990992.  
 Amendment dated December 1, 2016, filed December 22, 2016, as Document No. 101717.  
 11.) Subject to Easement for Ingress and egress purposes as created in Declaration of Easement dated October 27, 2008, filed October 28, 2008, as Document No. A81116. [shown on survey].  
 12.) Subject to non-exclusive cross easements as created in Cross Parking and Access Easement dated October 27, 2008, filed October 28, 2008, as Document No. A81120. [Easement area is not defined as the particular development of the property is unknown, and may vary substantially from the Exhibit A depiction attached to Doc. No. 81120.]  
 As Amended by Amendment to Cross Parking and Access Easement dated January 4, 2011, filed January 26, 2011, as Doc. No. A873533. [The "New Market Bank" parcel is shown for reference, Exhibit "A" which describes the "Third Party" parcel was not attached to the Document provided to us.]  
 13.) Subject to an Easement granted to Minnesota Utilities Co. as created in Easement Indenture dated October 30, 1942, filed August 29, 1944 in Book 104 of Deeds on pages 202-3. [Easement is blanket in nature].  
 As confined to the utility and roadway easements as shown on the plat of Elko Market Place as reserved in Partial Release of Easement dated August 29, 2011, filed September 9, 2011, as Doc. No. A887147. [shown on survey].  
 14.) Declaration and Restrictive Covenants for Elko Market Place dated December 30, 2010, filed January 28, 2011, as Doc. No. A873534. [Easements are blanket in nature].  
 15.) Subject to Easement for signage purposes over the Northeastern corner of subject premises as created in Declaration of Easements dated October 27, 2008, filed October 28, 2008, as Document No. A81115. [shown on survey].

**GENERAL NOTES**

- 1.) Survey coordinate basis: Scott County Coordinate System.
- 2.) "No Buildings Observed" on the surveyed property in the process of conducting the fieldwork.
- 3.) There are proposed changes in street right of way lines per the City of Elko New Market Planning Department. They will be addressed during the replating process. There was no evidence of recent street or sidewalk construction or repairs observed in the process of conducting the fieldwork.
- 4.) Wetland shown hereon is per delineation markers observed in the process of conducting the fieldwork.
- 5.) Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 21 of Table A, calls for sanitary sewers, storm sewer inverts, adjacent structures, sanitary, storm and water pipe sizes, street widths, street medians, adjacent and cross driveways to be shown.
- 6.) Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 22 of Table A, calls for 4 control points to be set and note the northing, easting and elevation of the points.

**UTILITY NOTES**

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call locates requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.
- 2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket number 192612731.
- 4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

**FLOOD ZONE NOTE**

- 1.) The subject property appears to lie within Zone X (Areas determined to be outside the 0.2% annual chance floodplain) per the National Flood Insurance Program, Flood Insurance Rate Map Community Flood No. 270643C0125C, dated February 19, 1987. The City of Elko New Market, Scott County, has annexed into Scott County FIRM panel 2704280125C, dated February 19, 1987. This information was obtained from the FEMA Map Service Center web site.

**ZONING NOTES**

- 1.) Zoning information and building setback depiction shown hereon is per zoning letter dated August 30, 2019 prepared by City of Elko New Market, Community Development Department.

Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, Item 8(c) of Table A specifies "current zoning classification, setback requirements, the height and floor space area restrictions, and parking requirements". Only those items set forth in said letter have been listed.

The property is currently zoned Planned Unit Development (PUD) and is subject to requirements of a Planned Unit Development Agreement and subsequent amendments (attached). The PUD Agreement states that the property is subject to the B5 zoning district requirements, except as otherwise stated in the PUD Agreement. Any future use within this commercial area requires an amendment to the PUD for the proposed use. Another option would be to rezone the property to a specific zoning district to remove it from the PUD.

Motor fuel stations and commercial car washes are listed as Conditional Use in the B5 district, performance standards relating to each are contained within the chapter. All other applicable sections of the Zoning Ordinance also apply. Below is a summary of miscellaneous requirements:

**Building setback requirements are as follows:**

- o From CS&H 2 - 65' from right-of-way line
- o From France Avenue - 65' from right-of-way line
- o Side Setback - 20' - from south property line
- o Rear Setback - 30' - from west property line

**Maximum building height - 35'**

No maximum floor space or impervious surface requirements, other than those dictated by the landscaping requirements.

**Parking requirements**

- o Convenience store - 1 space per 100 square feet of floor area

Zoning letter provided recites zoning code. Due to space limitations, said zoning letter is included herewith for reference.

**PARKING**  
None observed

**AREA**  
177,724 square feet or 4.080 acres

**SURVEYOR'S CERTIFICATION**

To Kwik Trip, Inc., a Wisconsin Corporation, First American Title Insurance Company dba Tri-County Abstract and Title Guaranty.

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 6(a), 6(b), 7(a), 8, 9, 11, 14, 17, 18, 19, 21 and 22 of Table A thereof. The fieldwork was completed on September 25, 2019.

Dated this 4th day of October, 2019.

SUNDE LAND SURVEYING, LLC.  
 By: *Arlee J. Carlson*  
 Arlee J. Carlson, P.L.S. Minn. Lic. No. 44900



Added note to Sch. B2 - Item 13 [JMD 10-23-2019]  
 Revision [ ] By [ ] Date [ ]

Drawing Title:  
**ALTA/NSPS LAND TITLE SURVEY FOR:  
 KWIK TRIP, INC.  
 STORE # XXX  
 NEW MARKET, MN**

**SUNDE LAND SURVEYING**  
 www.sundes.com  
 Main Office: 9001 East Bloomington Freeway (25W) • Suite 118  
 Bloomington, Minnesota 55420-3435  
 952-881-2455 (Fax: 952-888-9526)

Project: 2019-136 By/Pg: 1177/01 Date: 10/04/2019  
 Township: 113 Range: 21 Section: 27  
 File: 2019136001.dwg Sheet: 1 of 1

- LEGEND**
- Denotes found iron monument with plastic cap marked RLS 19086, unless otherwise noted.
  - AGP Denotes above ground pipe
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  - W Denotes water line
  - WV Denotes water valve
  - SHB Denotes shrub
  - TR Denotes deciduous tree

**BENCHMARKS (BM)**  
(NAVD 88)

- 1.) Top of post of fire hydrant on east side of Market Place Blvd. northwest of post office  
Elevation = 1136.78 feet
- 2.) Top of railroad spike in face of 1st power pole south of County Road No. 2 on west side of France Avenue.  
Elevation = 1133.74 feet
- 3.) Top of railroad spike in face of 3rd power pole south of County Road No. 2 on west side of France Avenue.  
Elevation = 1135.77 feet

**CONTROL STATIONS**

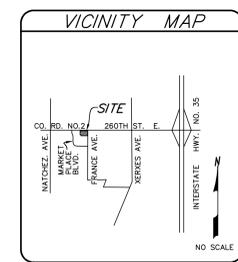
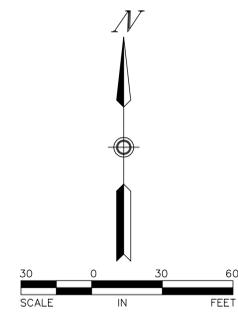
POINT	NORTHING	EASTING	ELEV.
3000	136889.249	496712.727	1135.22
3001	136876.829	496278.346	1137.49
4006	136432.857	496789.823	1134.29
4009	136433.746	496326.089	1134.86

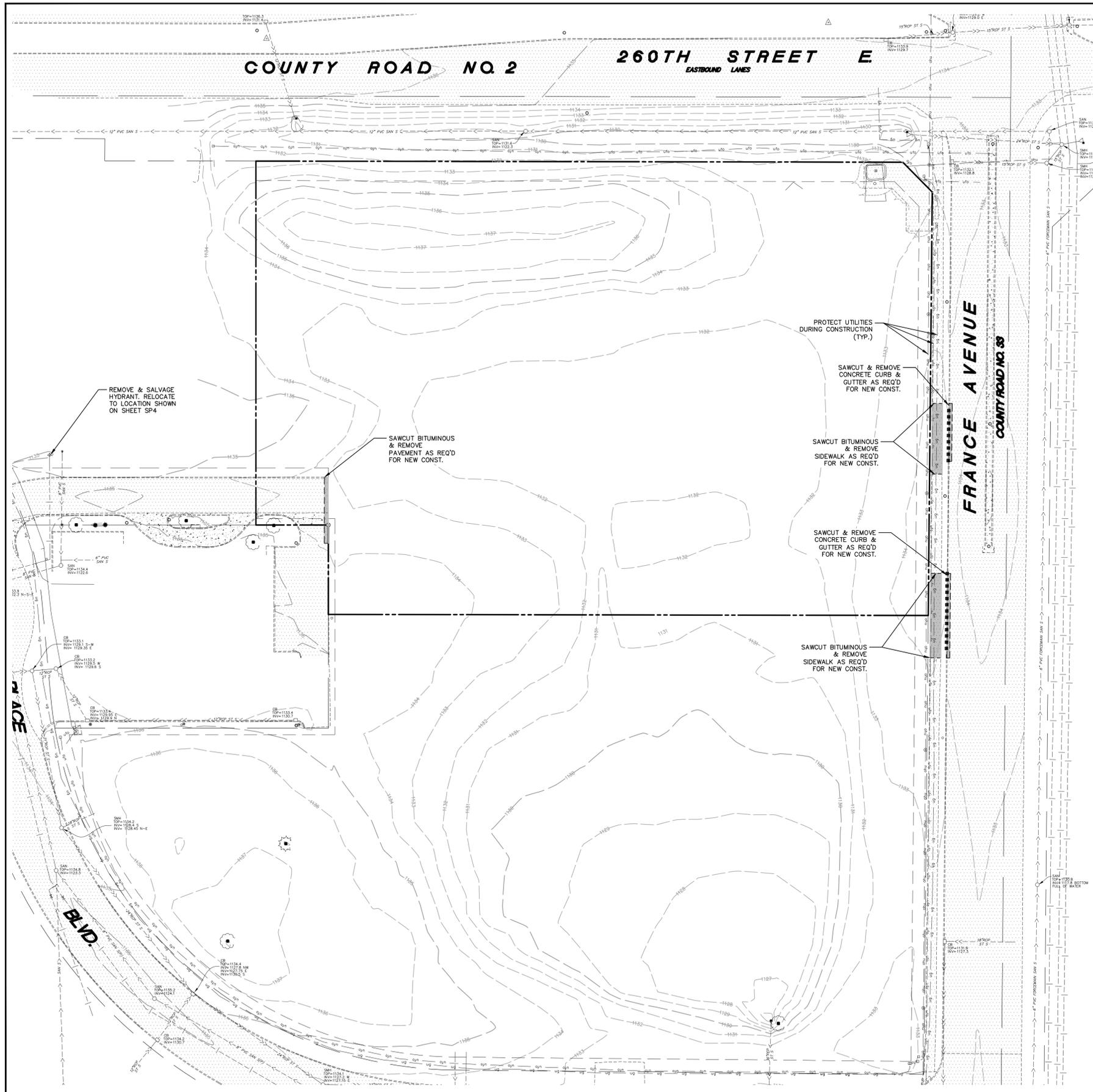
**LIST OF POSSIBLE ENCROACHMENTS**

The following list of possible encroachments is only the opinion of this surveyor; should not be interpreted as a legal opinion and should not be interpreted as a complete listing.

A) Possible encroachments are indicated on survey with boxed letters as listed below.

A.) Billboard outside of easement





**PLAN LEGEND**

EXISTING	REMOVAL
CURB	AS NOTED
BITUMINOUS CONCRETE PAVEMENT	
PROPERTY LINE	
EASEMENT LINE	
STORM SEWER	
SANITARY SEWER	
WATER MAIN	
UNDERGROUND GAS	
OVERHEAD UTILITY	
UNDERGROUND FIBER OPTIC	
SANITARY MANHOLE	
HYDRANT	
GATE VALVE	
CATCH BASIN	
LIGHT POLE	
SIGN	
GUARD POST	
GAS VALVE	
UTILITY POLE	

**EXISTING CONDITIONS NOTE**

1. EXISTING CONDITIONS ARE BASED ON ALTA/NSPS LAND TITLE SURVEY PREPARED BY SUNDE LAND SURVEYING, DATED 10/04/2019.

**REMOVAL PLAN NOTES**

1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LOCATION, DEPTH AND TYPES OF EXISTING UTILITIES AND TO NOTIFY THE OWNER AND ENGINEER IMMEDIATELY OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.

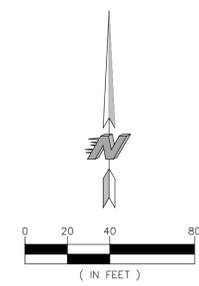
2. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING UTILITIES, APPURTENANCES AND STRUCTURES NOT INDICATED FOR REMOVAL. DAMAGE CAUSED BY DEMOLITION OPERATIONS SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.

3. CONTRACTOR TO REMOVE/RELOCATE EXISTING PRIVATE UTILITIES AS NECESSARY. THE CONTRACTOR SHALL COORDINATE THESE ACTIVITIES WITH THE UTILITY COMPANIES.

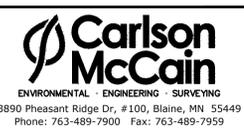
4. THE CONTRACTOR IS RESPONSIBLE FOR ALL THE REMOVALS SHOWN ON THE PLANS AND SHALL CONFORM/ADHERE TO ALL GOVERNING STATE AND LOCAL REGULATIONS. ALL PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.

**BENCHMARKS**

1. TOP OF NUT OF FIRE HYDRANT ON EAST SIDE OF MARKET PLACE BLVD NORTHWEST OF POST OFFICE.  
ELEVATION = 1136.78 (NAVD 88)



**KWIK TRIP, Inc.**  
P.O. BOX 2107  
1626 OAK STREET  
LA CROSSE, WI 54602-2107  
PH. (608) 781-8988  
FAX (608) 781-8960



I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Name: Joseph T. Radach, P.E.  
Signature: *[Signature]*  
Date: 05/15/20 License #: 45889

**EXISTING CONDITIONS & REMOVALS PLAN**

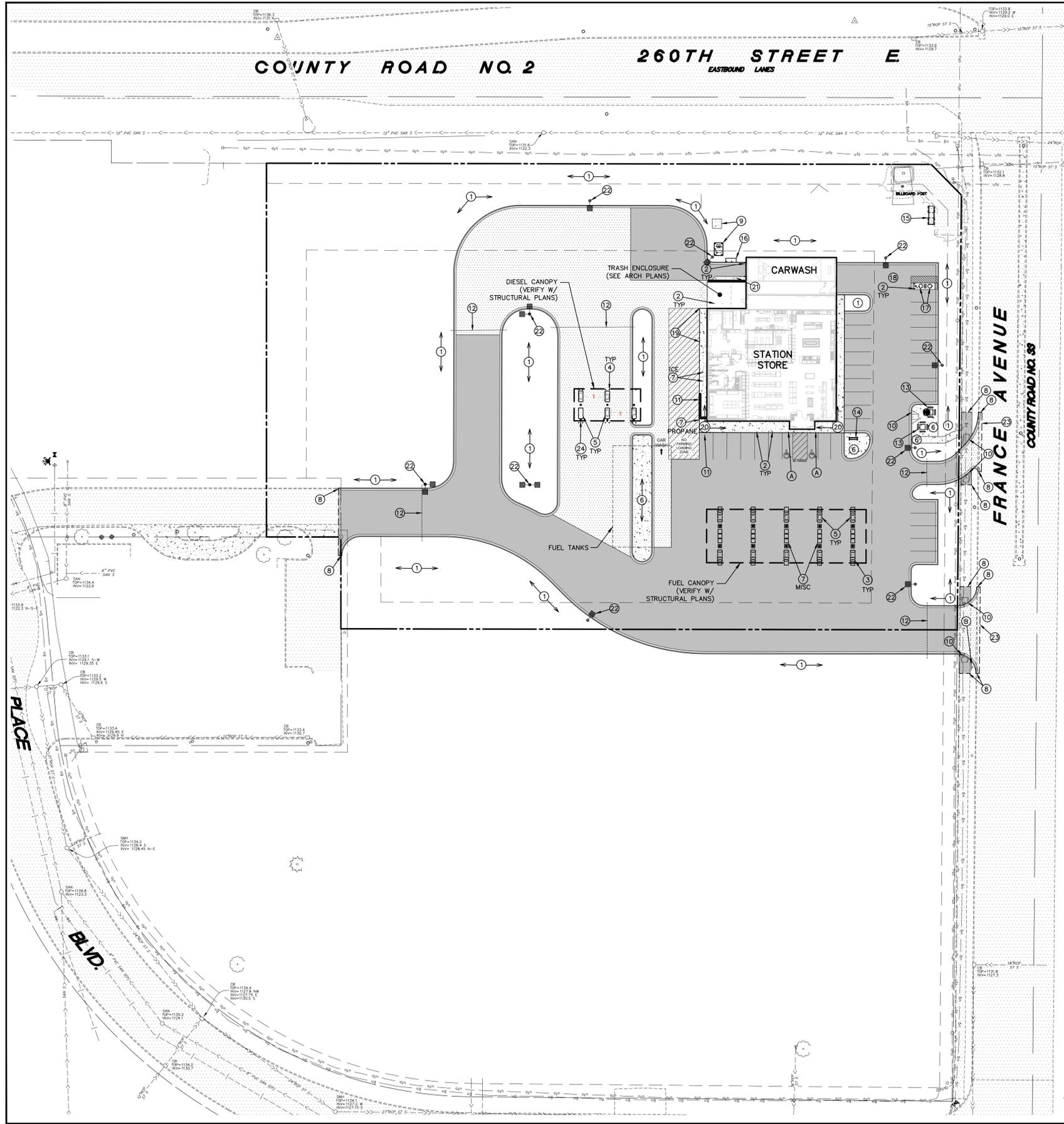
**CONVENIENCE STORE #1116 WITH SINGLE BAY CARWASH AND SIDE DIESEL**

**260TH ST E & FRANCE AVE**  
**ELKO NEW MARKET, MINNESOTA**

#	DATE	DESCRIPTION

DRAWN BY: JTR  
SCALE: GRAPHIC  
PROJ. NO.: 8622-00  
DATE: 2020-05-15  
SHEET: 1116 SP1





**PLAN LEGEND**

EXISTING	PROPOSED
CURB	HEAVY DUTY 25,330 SF
BITUMINOUS	LIGHT DUTY 57,455 SF
CONCRETE PAVEMENT	
CONCRETE WALK	
PROPERTY LINE	
EASEMENT LINE	
STORM SEWER	
SANITARY SEWER	
WATER MAIN	
UNDERGROUND GAS	
OVERHEAD UTILITY	
UNDERGROUND FIBER OPTIC	
SANITARY MANHOLE	
HYDRANT	
GATE VALVE	
CATCH BASIN	
LIGHT POLE	
SIGN	
GUARD POST	
GAS VALVE	
UTILITY POLE	

**PLAN KEYNOTES**

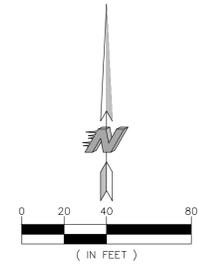
- LANDSCAPE AREA. SEE SHEET L1.
- 30" HEIGHT, 6" DIAMETER CONCRETE FILLED PIPE BOLLARD. SEE DETAIL 6/SP6.
- 36" HEIGHT, 6" DIAMETER CONCRETE FILLED PIPE BOLLARD. SEE DETAIL 7/SP6.
- 48" HEIGHT, 6" DIAMETER CONCRETE FILLED PIPE BOLLARD. SEE DETAIL 8/SP6.
- CONCRETE ISLAND WITH 6" EXPOSURE WITH FUEL DISPENSER. DISPENSER PER OWNER.
- 4" CONCRETE WALK.
- OUTDOOR MERCHANDISING.
- MATCH EXISTING CURB & GUTTER/SIDEWALK/PAVEMENT.
- TRANSFORMER LOCATION.
- PEDESTRIAN CURB RAMP, SEE SHEET SP8
- TOTE SCREENING WALL. SEE ARCHITECTURAL PLANS.
- PVC IRRIGATION SLEEVE UNDER PAVEMENT. VERIFY WITH IRRIGATION PLAN FOR EXACT SIZE AND LOCATION BEFORE INSTALLATION.
- PICNIC TABLE PROVIDED BY OWNER
- BIKE RACK PER OWNER.
- PYLON SIGN.
- CARWASH KEYPAD PER OWNER. PROVIDE TRASH CONTAINER PER OWNER.
- AUTO VACUUM PER OWNER ON CONCRETE ISLAND WITH 6" EXPOSURE. PROVIDE TRASH CONTAINER PER OWNER.
- "FREE AIR" COMPRESSOR PER OWNER. PROVIDE SIGNAGE PER OWNER.
- PAINT FACE OF CURB TRANSITION WITH HIGH VISIBILITY YELLOW PAINT.
- 6" INTEGRAL CONCRETE WALK/CURB.
- CONCRETE CURB ISLAND.
- SITE AREA LIGHT WITH CONCRETE BASE PER DETAIL 2/SP6.
- CONCRETE VALLEY GUTTER PER DETAIL 12/SP7.
- DIESEL TRUCK AIR MACHINE.

**SIGNAGE & STRIPING NOTES**

- ALL SIGNS SHALL BE PLACED 18" MINIMUM BEHIND CURB UNLESS OTHERWISE NOTED.
  - SIGNAGE SHALL INCLUDE SIGN, POST, HARDWARE, CONCRETE FOOTING AND STEEL CASING (IF REQUIRED).
  - PARKING LOT STRIPING SHALL BE 4" HIGH VISIBILITY SOLID YELLOW PAINT.
  - PAVEMENT HATCHING, LANE MARKINGS AND TEXT SHALL BE 4" HIGH VISIBILITY PAINT.
- COLORS: HANDICAP - BLUE  
ALL OTHERS - YELLOW
- HANDICAP PARKING PROVIDED PER ADA CODE.

**SIGN SCHEDULE (PER W MUTCD)**

- (A) HANDICAP PVC BOLLARD SLEEVE PER OWNER. SEE DETAIL 4/SP7.
- (B) STOP SIGN: R1-1 (30"x30")



**BENCHMARKS**

- TOP OF NUT OF FIRE HYDRANT ON EAST SIDE OF MARKET PLACE BLVD NORTHWEST OF POST OFFICE.  
ELEVATION = 1136.78 (NAVD 88)



**Kwik Trip**

**Kwik Star**

KWIK TRIP, Inc.  
P.O. BOX 2107  
1626 OAK STREET  
LA CROSSE, WI 54602-2107  
PH. (608) 781-8988  
FAX (608) 781-8960

**Carlson McCain**  
ENVIRONMENTAL - ENGINEERING - SURVEYING  
3890 Pheasant Ridge Dr, #100, Blaine, MN 55449  
Phone: 763-489-7900 Fax: 763-489-7959

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Name: Joseph T. Radach, P.E.  
Signature: [Signature]  
Date: 05/15/20 License #: 45889

**SITE PLAN (KEYNOTE)**

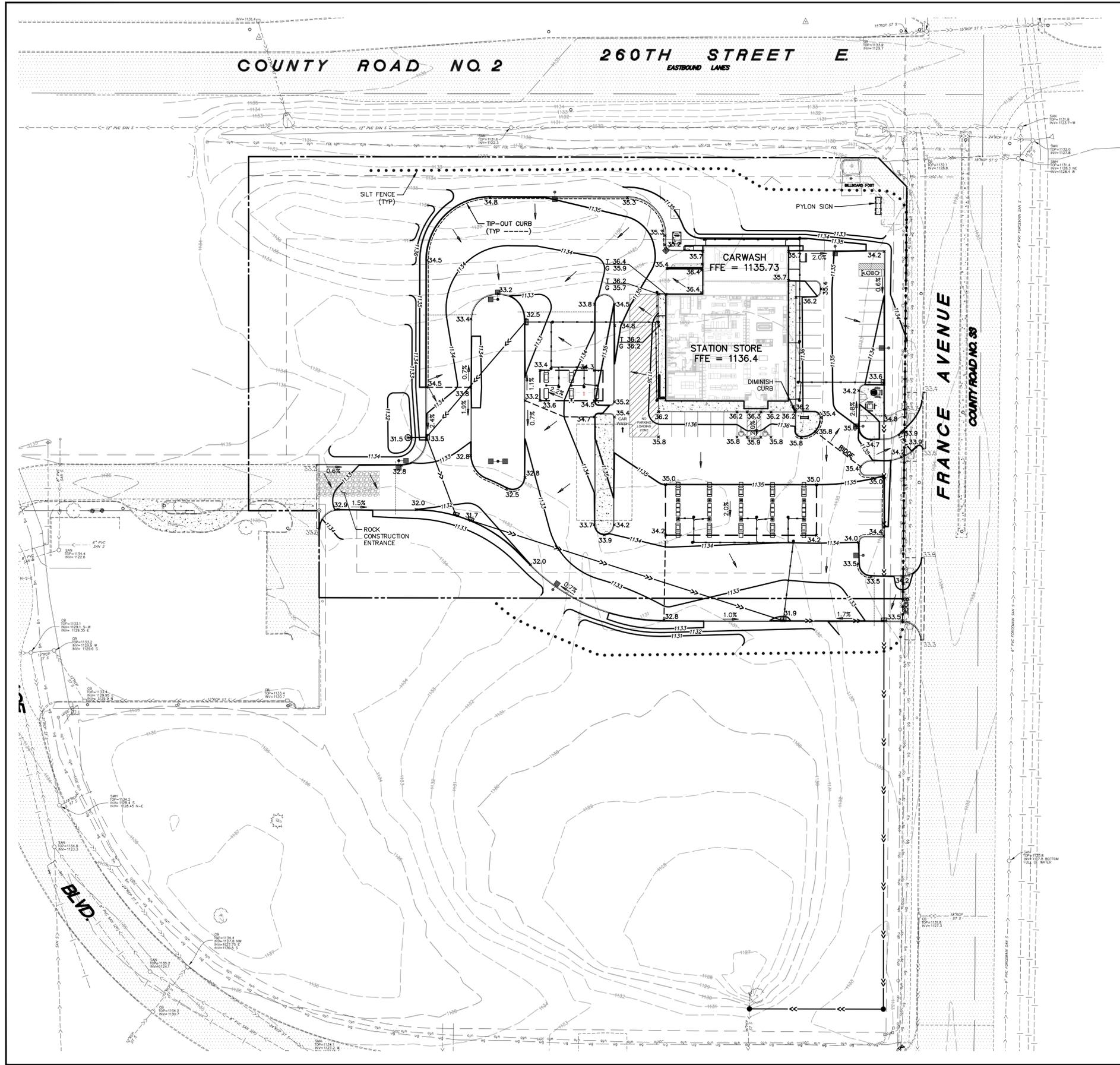
**CONVENIENCE STORE #1116 WITH SINGLE BAY CARWASH AND SIDE DIESEL**

**260TH ST E & FRANCE AVE**  
**ELKO NEW MARKET, MINNESOTA**

#	DATE	DESCRIPTION

DRAWN BY: JTR  
SCALE: GRAPHIC  
PROJ. NO: 8622-00  
DATE: 2020-05-15  
SHEET: 1116 SP2.1





**PLAN LEGEND**

EXISTING	PROPOSED
CURB	
BITUMINOUS CONCRETE PAVEMENT	
CONCRETE WALK	
PROPERTY LINE	
EASEMENT LINE	
STORM SEWER	
SANITARY SEWER	
WATER MAIN	
UNDERGROUND GAS	
OVERHEAD UTILITY	
UNDERGROUND FIBER OPTIC	
SANITARY MANHOLE	
HYDRANT	
GATE VALVE	
CATCH BASIN	
LIGHT POLE	
SIGN	
GUARD POST	
GAS VALVE	
UTILITY POLE	
5' CONTOUR	
1' CONTOUR	
SPOT ELEVATION (CURB ELEVATIONS ARE TO GUTTER LINE)	
SILT FENCE	

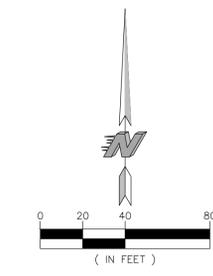
- GOVERNING SPECIFICATIONS**
- THE LATEST EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION".
  - THE LATEST EDITION OF THE CITY OF ELKO NEW MARKET STANDARD DETAILS AND SPECIFICATIONS.
  - THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD).
  - THE LATEST EDITION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD SPECIFICATIONS.

- GRADING NOTES**
- SILT FENCE AND EXISTING CATCH BASIN INLET PROTECTION SHALL BE INSTALLED PRIOR TO GRADING CONSTRUCTION, AND SHALL BE MAINTAINED UNTIL SITE HAS BEEN STABILIZED.
  - CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND PAVEMENTS PRIOR TO THE START OF GRADING CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF DISCREPANCIES OR VARIATIONS FROM THE PLAN.
  - CONTRACTOR SHALL STRIP, STOCKPILE AND RESPREAD SUFFICIENT TOPSOIL TO PROVIDE A MINIMUM OF 4" OF TOPSOIL OVER ALL DISTURBED AREAS THAT WILL BE SODDED, SEEDED OR LANDSCAPED.

**BENCHMARKS**

1. TOP OF NUT OF FIRE HYDRANT ON EAST SIDE OF MARKET PLACE BLVD NORTHWEST OF POST OFFICE.

ELEVATION = 1136.78 (NAVD 88)



CALL BEFORE YOU DIG

**811**

Know what's below. Call before you dig.



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Name: Joseph T. Radach, P.E.  
Signature: *J. T. Radach*  
Date: 05/15/20 License #: 45889

**GRADING & EROSION CONTROL PLAN**

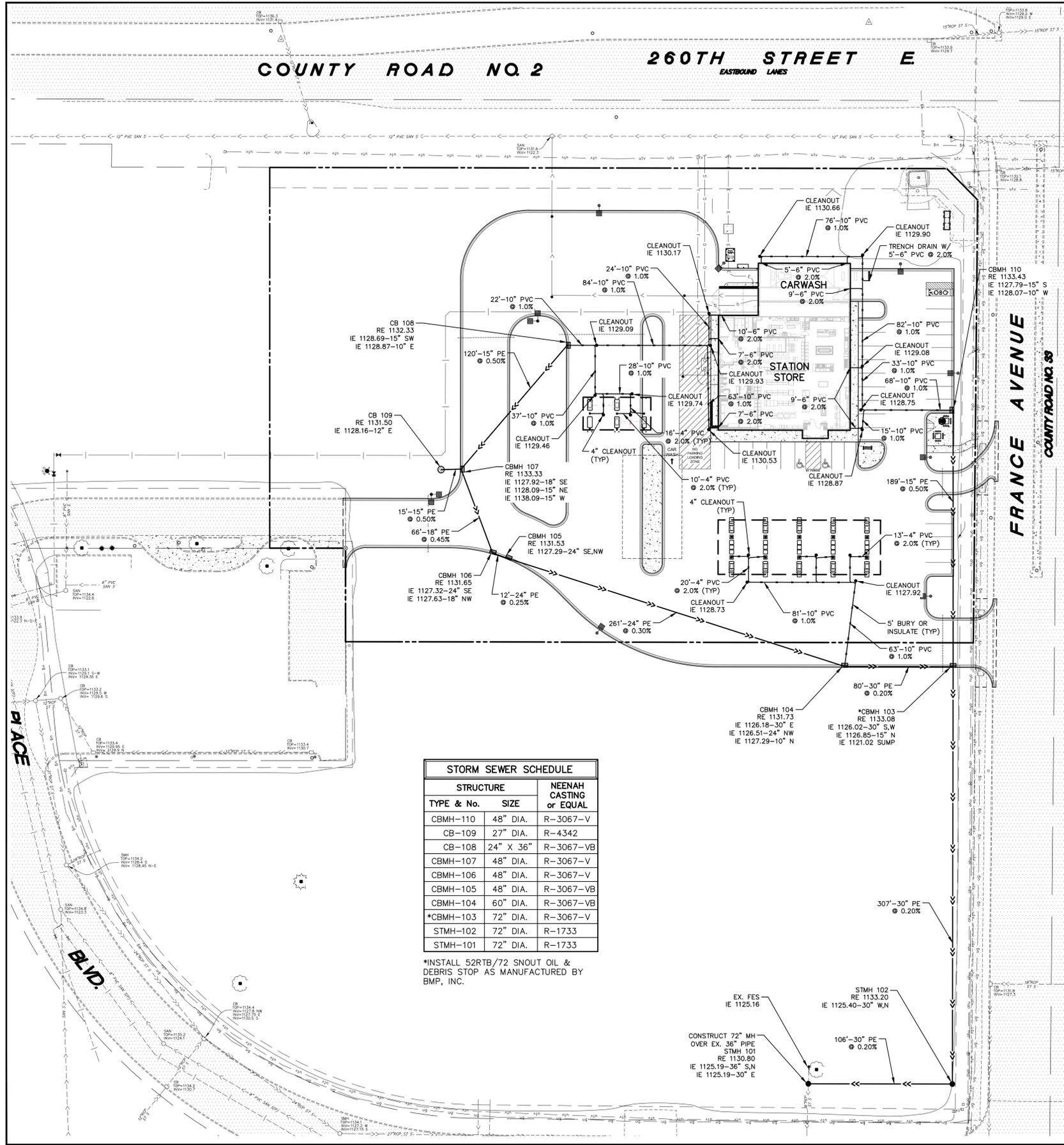
**CONVENIENCE STORE #1116 WITH SINGLE BAY CARWASH AND SIDE DIESEL**

260TH ST E & FRANCE AVE  
ELKO NEW MARKET, MINNESOTA

#	DATE	DESCRIPTION

DRAWN BY: JTR  
SCALE: GRAPHIC  
PROJ. NO.: 8622-00  
DATE: 2020-05-15  
SHEET: 1116 SP3





**PLAN LEGEND**

EXISTING	PROPOSED
CURB	
BITUMINOUS CONCRETE PAVEMENT	
CONCRETE WALK	
PROPERTY LINE	
EASEMENT LINE	
STORM SEWER	
SANITARY SEWER	
WATER MAIN	
UNDERGROUND GAS	
OVERHEAD UTILITY	
UNDERGROUND FIBER OPTIC	
UNDERGROUND ELECTRIC	
SANITARY MANHOLE	
HYDRANT	
GATE VALVE	
CATCH BASIN	
LIGHT POLE	
SIGN	
GUARD POST	
GAS VALVE	
UTILITY POLE	

**BENCHMARKS**  
 1. TOP OF NUT OF FIRE HYDRANT ON EAST SIDE OF MARKET PLACE BLVD NORTHWEST OF POST OFFICE.  
 ELEVATION = 1136.78 (NAVD 88)



- GENERAL:**
- Governing Specifications: The latest editions of the Minnesota Department of Transportation "Standard Specifications for Construction", the City Engineers Association of Minnesota (CEAM) Standard Specifications, the City of Elk New Market Standard Specifications and the Minnesota Plumbing Code.
  - Comply with the work safety practices specified by the Occupational Safety and Health Administration (OSHA). Comply with all applicable local, state, and federal safety regulations. OSHA prohibits entry into "confined spaces", such as manhole and vaults (see 29CFR Section 1910.146) without undertaking certain specific practices and procedures. Construction safety is solely the responsibility of the Contractor, who is also safety responsible for the means, methods, and sequencing of the construction operations.
  - Perform all utility work in accordance with State and City requirements.
  - Contact the City of Elk New Market Public Works Department for inspection of all utility work.
  - Connect to existing sanitary sewer MH's by coring/drilling. Connect to existing storm sewer MH's by either sawcutting or coring/drilling. Meet all City standards and specifications for the connection. Reconstruct vaults after installation. Use water stop gaskets in order to provide watertight seals when penetrating a structure wall with a pipe. Take measures before beginning construction to ensure that service connections do not cut into maintenance access structure joints or pipe barrel joints.
  - Perform trench excavations for all utilities in accordance with the requirements of OSHA 29 CFR, Part 1926, Subpart P, "Excavations and Trenches". (www.osha.gov)
  - Coordinate utility location locations at 5 ft. out from the proposed building with the interior plumbing contractor prior to construction. Verify water and sewer service locations and elevations with the Mechanical Engineer prior to construction.
  - The locations of existing utilities shown on this plan are from record information and field as-built data. The Engineer does not guarantee that all existing utilities are shown or, if shown, exist in the locations indicated on the plan. It is the Contractor's responsibility to verify the vertical and horizontal location of all existing utilities (including water service lines and appurtenances). Notify the Engineer of any discrepancies.
  - Contact utility companies for locations of all public and private utilities within the work area prior to beginning construction. Contact 811 for locations of existing utilities at least 72 hours (not including weekends or holidays) before beginning any construction. Obtain ticket number and meet with representatives of the various utilities at the site. Provide the Owner with the ticket number information.
  - Patrol to verify the location of existing underground facilities at a sufficient number of locations in order to assure that no conflict with the proposed work exists and that sufficient clearance is available.
  - Where existing gas, electric, cable, or telephone utilities conflict with the work, coordinate the abandonment, relocation, offset, or support of the existing utilities with the appropriate utility companies. Coordinate new gas meter and gas line installation, electric meter and electric service installation, cable service and telephone service installation with the local utility companies.
  - Arrange for and secure suitable disposal areas off-site. Dispose of all excess soil, waste material, debris, and all materials not designated for salvage. Waste material and debris include trees, stumps, pipe, concrete, asphaltic concrete, cans, or other waste material from the construction operation. Obtain the rights to any waste area for disposal of unsalvageable or surplus material either shown or not shown on the plans. All work in disposing of such material shall be considered incidental to the work. All disposal must conform to applicable solid waste disposal permit regulations. Obtain all necessary permits at no cost to the Owner.
  - Straight line saw-cut existing bituminous or concrete surfacing at the perimeter of pavement removal areas. Use saws that provide water to the blade. Tack and match all connections to existing bituminous pavement.
  - Relocate overhead power, telephone, and cable lines as required.
  - All materials required for this work shall be new material conforming to the requirements for class, kind, grade, size, quality, and other details specified herein or as shown on the Plans. Do not use recycled or salvaged aggregate, asphaltic pavement, crushed concrete, or scrap shingles. Unless otherwise indicated, the Contractor shall furnish all required materials.
  - Restore the public right-of-way. Restore any concrete curb and gutter, bituminous pavement, sidewalk, or vegetative cover damaged by the construction activity. Restore damaged turf with sod within the public right-of-way. The work area shown is general and may need to be adjusted in the field.
  - When sawing or drilling concrete or masonry, use saws that provide water to the blade. Do not allow the slurry produced by this process to be tracked outside of the immediate work area or discharged into the sewer system.
  - Adjust all curb stops, valve boxes, maintenance hole castings, catch basin castings, cleanout covers, and similar items to finished grade.
  - Install all pipe with the ASTM identification numbers on the top for inspection. Commence pipe laying at the lowest point in the proposed sewer line. Lay the pipe with the bell end or rearing groove end of the pipe pointing upward. When connecting to an existing pipe, uncover the existing pipe in order to allow any adjustments in the proposed line and grade before laying any pipe.
  - Obtain and pay for all permits, tests, inspections, etc. required by agencies that have jurisdiction over the project. The Contractor is responsible for all bonds, letters of credit, or cash sureties related to the work. Execute and inspect work in accordance with all local and state codes, rules, ordinances, and regulations pertaining to the particular type of work involved.
- STORM DRAINAGE:**
- Unless otherwise indicated, use reinforced, precast, concrete maintenance holes and catch basins conforming to ASTM C478, furnished with water stop gaskets and precast bases. Joints for all precast maintenance hole sections shall have confined, rubber "O"-ring gaskets in accordance with ASTM C923. The inside barrel diameter shall not be less than 48 inches, unless otherwise noted.
  - All joints and connections to catch basins or manholes shall be watertight. Use resilient rubber seals, waterstop gaskets, or approved equal. Cement mortar joints are not allowed, unless otherwise permitted by local municipality.
  - Maintenance holes and catch basins in paved areas shall be sumped 0.05 feet. Catch basins in the gutter line shall be sumped 0.17 feet. Elevations shown on the plans represent the sumped elevations.
  - PVC Pipe: Use solid-core, SDR-35, ASTM D3034 Polyvinyl Chloride (PVC) Pipe for designated PVC storm sewer 4 to 10-inches in diameter, unless otherwise noted. Joints for all PVC storm sewer shall have push-on joints with molded rubber gaskets meeting the requirements of ASTM D3212 except for service lateral pipe connections to pipes, tees, wyes, and fittings which shall have solvent-cemented joints meeting the requirements of ASTM D2885 and ASTM D3212. Lay all PVC pipe on a continuous granular bed. Installation must comply with ASTM D3232.
  - RC Pipe: Reinforced concrete pipe (RCP) and fittings shall conform to ASTM C76, Design C, with circular reinforcing for the class of pipe specified. Use Class IV RCP for pipes 21" and larger. Use Class V RCP for pipes 18" and smaller. Joints shall be Bureau of Reclamation type R-4, with confined rubber "O"-ring gaskets in accordance with ASTM C361.
  - RC Aprons: Install a reinforced concrete apron on the free end of all daylight RCP storm sewer pipes. Tie the last three sections (including apron) of all daylight RCP storm sewers with a minimum of two tie bolt fasteners per joint. This requirement applies to both upstream and downstream pipe inlets and outlets. For concrete culverts, tie all joints. Ties to be used only to hold the pipe sections together, not for pulling the sections tight. Install safety-truss rocks on all concrete aprons.
  - Testing: Test all portions of storm sewer that are within 10 feet of buildings, within 10 feet of buried water lines, within 50 feet of water wells, or that pass through soil or water identified as being contaminated. Test all flexible storm sewer lines for deflection after the sewer line has been installed and backfill has been in place for at least 30 days. No pipe shall exceed a deflection of 3%. If the test fails, make necessary repairs and retest.
  - Use Neenah castings, or approved equal, of the type indicated in the Storm Sewer Schedule.
  - The minimum depth of cover for building and canopy roof drain leaders without insulation is 5 feet. Insulate roof drain leaders at locations where the depth of cover is less than 5 feet. Provide a minimum insulation thickness of 2 inches. The insulation must be at least 4 feet wide and centered on the pipe. Install the insulation boards 6 inches above the tops of the pipes on mechanically compacted and leveled pipe bedding material. Use high density, closed cell, rigid board equivalent to DOW Styrofoam HI-40 plastic foam insulation.
  - Cleanouts: Install cleanouts on all roof drains in accordance with S.P.S. 382.35 (3)(C)(1). The distance between cleanouts in horizontal piping shall not exceed 100 feet per pipes 10-inches and under in size. Cleanouts shall be of the same nominal size as the pipes they serve. Install a meter box frame and solid lid (Neenah R-1914-A, or approved equal) over all cleanouts unless otherwise noted.
  - HDPE Requirements:
    - Install dual-wall, smooth interior, corrugated high-density polyethylene (HDPE) pipe at locations indicated on the plan.
    - HDPE pipe shall conform to the requirements of AASHTO M294 and Section 12 of the AASHTO LRFD Bridge Design Specification.
    - Pipes 4"-10" in size must comply with AASHTO M252.
    - Pipes 12"-60" in size must comply with ASTM F2306.
    - All fittings must comply with ASTM Standard D3212.
    - Water-tight joints must be used at all connections including structures.
    - Lay all HDPE pipe on a continuous granular bed. Installation must comply with ASTM D3231. All sections of the corrugated HDPE pipe shall be coupled in order to provide water tight joints.
    - Testing shall be performed in accordance with the City specifications.

STORM SEWER SCHEDULE		
STRUCTURE	SIZE	NEENAH CASTING OR EQUAL
CBMH-110	48" DIA.	R-3067-V
CB-109	27" DIA.	R-4342
CB-108	24" X 36"	R-3067-VB
CBMH-107	48" DIA.	R-3067-V
CBMH-106	48" DIA.	R-3067-V
CBMH-105	48" DIA.	R-3067-VB
CBMH-104	60" DIA.	R-3067-VB
*CBMH-103	72" DIA.	R-3067-V
STMH-102	72" DIA.	R-1733
STMH-101	72" DIA.	R-1733

\*INSTALL 52RTB/72 SNOOT OIL & DEBRIS STOP AS MANUFACTURED BY BMP, INC.



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 P.O. BOX 2107  
 1626 OAK STREET  
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 Name: Joseph T. Radach, P.E.  
 Signature: [Signature]  
 Date: 05/15/20 License #: 45889

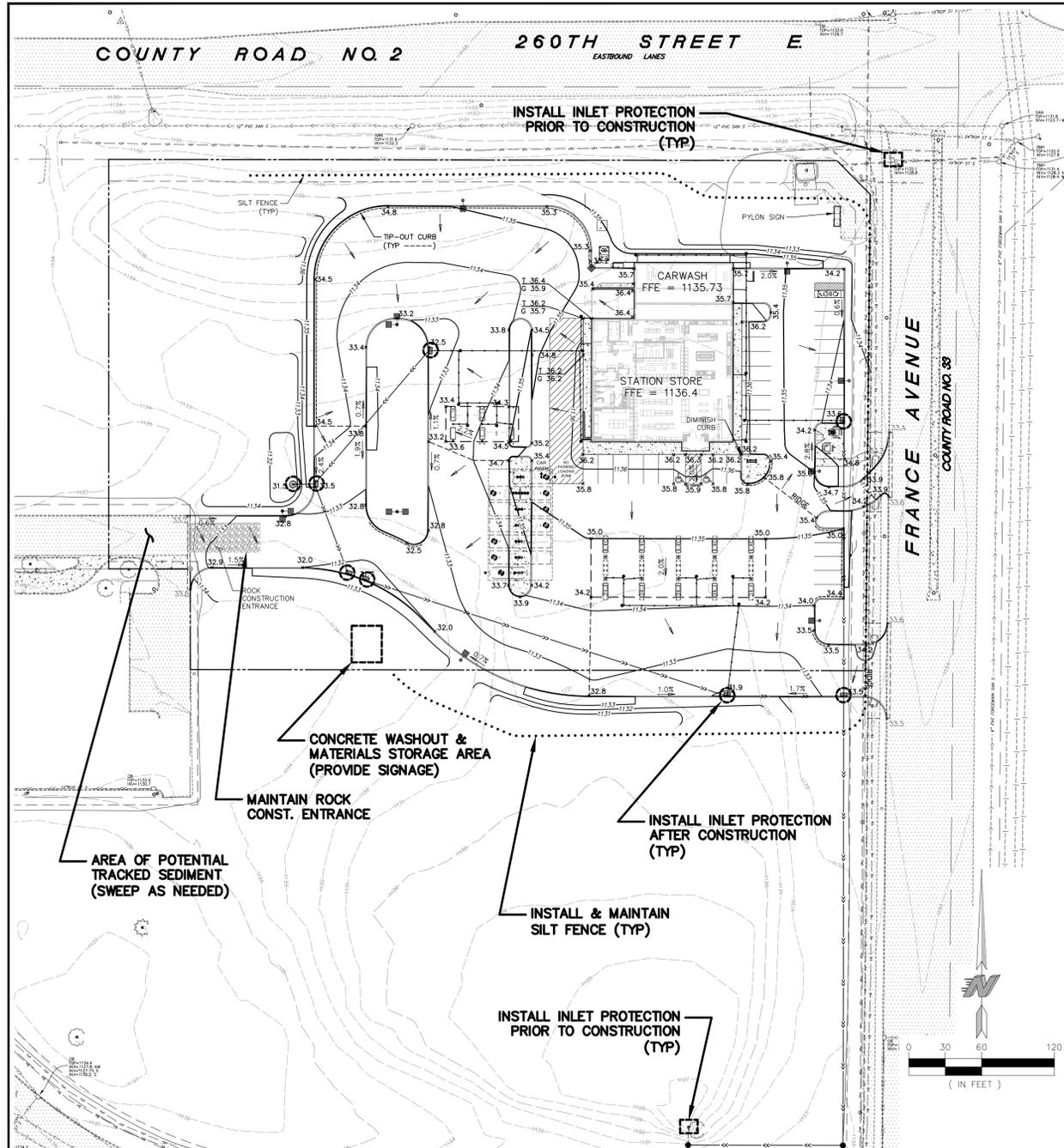
**UTILITY PLAN (STORM SEWER)**

**CONVENIENCE STORE #1116 WITH SINGLE BAY CARWASH AND SIDE DIESEL**

260TH ST E & FRANCE AVE  
 ELKO NEW MARKET, MINNESOTA

#	DATE	DESCRIPTION

DRAWN BY: JTR  
 SCALE: GRAPHIC  
 PROJ. NO: 8622-00  
 DATE: 2020-05-15  
 SHEET: 1116 SP4.1



**STORMWATER MANAGEMENT PLAN**  
 ACCORDING TO CONVERSATIONS WITH THE CITY OF ELKO NEW MARKET, THE EXISTING REGIONAL POND LOCATED SOUTH OF THE SITE WAS DESIGNED TO PROVIDE WATER QUALITY TREATMENT AND RATE CONTROL FOR THE ENTIRE ELKO MARKET PLACE IN A FULLY DEVELOPED CONDITION. IT IS TYPICAL TO DESIGN REGIONAL PONDS FOR COMMERCIAL DEVELOPMENT BASED ON AN ASSUMED IMPERVIOUSNESS OF 75 PERCENT. THE PROPOSED LAYOUT FOR THE KWIK TRIP SITE CONTAINS 2.34 ACRES OF IMPERVIOUS SURFACE, WHICH IS APPROXIMATELY 57 PERCENT OF THE AREA OF THE SITE. BECAUSE THE PROPOSED IMPERVIOUSNESS OF THE KWIK TRIP SITE IS WELL BELOW TYPICAL COMMERCIAL DEVELOPMENT, IT IS OUR OPINION THAT THE REGIONAL POND WILL PROVIDE ADEQUATE WATER QUALITY TREATMENT AND RATE CONTROL FOR THE SITE. THE ELKO NEW MARKET STORMWATER MANAGEMENT PLAN WAS NOT AVAILABLE FOR REVIEW AT THE TIME OF THIS REPORT. THE ELKO NEW MARKET STORMWATER MANAGEMENT PLAN SHOULD BE REVIEWED TO DETERMINE ACTUAL DESIGN PARAMETERS AND TO VERIFY THE REGIONAL POND CAPACITY.

THE MPCA CONSTRUCTION STORMWATER PERMIT REQUIRES SITES TO CONSIDER VOLUME REDUCTION PRACTICES WHERE PRACTICAL. THE MPCA CONSTRUCTION STORMWATER PERMIT PROHIBITS THE CONSTRUCTION OF INFILTRATION SYSTEMS THAT RECEIVE RUNOFF FROM VEHICLE FUELING AREAS. THE MPCA CONSTRUCTION STORMWATER PERMIT ALSO PROHIBITS INFILTRATION OF STORMWATER IN AREAS OF PREDOMINANTLY HYDROLOGIC SOIL GROUP "D". THE PROPOSED SITE WILL HAVE VEHICLE FUELING ONSITE AND ONSITE SOILS CONSIST PRIMARILY OF CLAY, THEREFORE INFILTRATION IS PROHIBITED ON SITE.

**PRIOR TO START OF CONSTRUCTION**  
 THE FOLLOWING STORMWATER POLLUTION PREVENTION MEASURES SHALL BE IMPLEMENTED PRIOR TO CONSTRUCTION. REFER TO GRADING AND EROSION CONTROL PLANS FOR LOCATIONS.

- SILT FENCE**  
SILT FENCE SHALL BE INSTALLED AT THE LIMIT OF GRADING ON ANY FILL SLOPE. ADDITIONAL SILT FENCE MAY BE REQUIRED IN CUT SLOPE AREAS. SILT FENCE SHALL ALSO BE INSTALLED AROUND ANY INFILTRATION/FILTRATION PRACTICE.
- ROCK CONSTRUCTION ENTRANCE**  
ROCK CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE FIELD ENTRANCES TO THE SITE.
- CATCH BASINS**  
ALL CATCH BASINS SHALL BE PROTECTED WITH INLET PROTECTION DEVICES APPROVED BY THE LOCAL GOVERNING UNIT. THESE SHALL INCLUDE, BUT ARE NOT LIMITED TO, WMCO PROTECTION DEVICES, INFRASAFE PROTECTION DEVICES, FILTER FABRIC, BIO ROLLS AND STRAW BALES.

**DURING CONSTRUCTION**  
 THE FOLLOWING STORMWATER POLLUTION PREVENTION MEASURES SHALL BE IMPLEMENTED DURING CONSTRUCTION. REFER TO GRADING AND EROSION CONTROL PLANS FOR LOCATIONS.

- PHASED GRADING**  
TO THE EXTENT POSSIBLE, GRADING SHALL BE PHASED TO MINIMIZE THE AMOUNT OF DISTURBED AREAS DURING SITE CONSTRUCTION.
- TRACKED SEDIMENT**  
ANY SEDIMENT TRACKED FROM THE SITE ONTO THE STREET SHALL BE REMOVED IMMEDIATELY UPON DETECTION. THE ROCK CONSTRUCTION ENTRANCE SHALL BE INSPECTED AND REPAIRED IF INUNDATED WITH SEDIMENT.
- STOCKPILES**  
STOCKPILES SHALL BE PLACED IN AN AREA THAT WILL MINIMIZE THE NEED FOR RELOCATION. IF A STOCKPILE WILL REMAIN IN PLACE FOR AN EXTENDED PERIOD OF TIME, STABILIZATION MEASURES SHALL BE IMPLEMENTED, INCLUDING BUT NOT LIMITED TO, SEEDING AND SILT FENCING. TEMPORARY STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS AND CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS, CONDUITS OR DITCHES.
- TOPSOIL**  
UPON GRADING COMPLETION, A MINIMUM OF 4 INCHES OF TOPSOIL SHALL BE PLACED OVER ALL DISTURBED AREAS, EXCLUDING PROPOSED STREETS AND PARKING AREAS.
- RESTORATION**  
DISTURBED AREAS NOT ACTIVELY WORKED SHALL BE RESTORED WITH SEED AND MULCH, EROSION CONTROL BLANKET AND/OR SOD WITHIN 7 DAYS.
- SLOPES**  
IN ORDER TO MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES, THERE SHALL BE NO UNBROKEN SLOPE LENGTH OF GREATER THAN 75 FEET FOR SLOPES WITH A GRADE OF 3:1 OR STEEPER.
- DRAINAGE DITCHES**  
THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH THAT DRAINS WATER FROM THE SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 200 LINEAL FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE TO ANY SURFACE WATER. STABILIZATION MUST BE COMPLETED WITHIN 24 HOURS OF CONNECTING TO A SURFACE WATER.
- PIPE OUTLETS**  
PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS OF CONNECTION TO A SURFACE WATER.
- CATCH BASINS**  
ALL CATCH BASINS SHALL BE PROTECTED WITH INLET PROTECTION DEVICES APPROVED BY THE LOCAL GOVERNING UNIT. THESE SHALL INCLUDE, BUT ARE NOT LIMITED TO, WMCO PROTECTION DEVICES, INFRASAFE PROTECTION DEVICES, FILTER FABRIC, BIO ROLLS AND STRAW BALES.
- DUST**  
CONSTRUCTION DUST SHALL BE CONTAINED TO THE EXTENT POSSIBLE. IF THE SITE BECOMES EXCESSIVELY DUSTY, APPROPRIATE MEASURES SHALL BE TAKEN TO REDUCE DUST BEING TRANSPORTED FROM THE SITE. DUST CONTROL MEASURES INCLUDE, BUT ARE NOT LIMITED TO, WATERING AND CALCIUM CHLORIDE APPLICATION.
- DEWATERING**  
DEWATERING ACTIVITIES SHALL BE CONDUCTED WITH AND APPROVED BY THE LOCAL GOVERNING UNIT. IF THERE WILL BE ANY DEWATERING OR BASIN DRAINING THAT MAY HAVE TURBID OR SEDIMENT LADEN DISCHARGE, THE WATER MUST BE DISCHARGED TO A TEMPORARY OR PERMANENT SEDIMENTATION BASIN ON THE PROJECT SITE WHENEVER POSSIBLE. APPROPRIATE BMPs SHALL BE USED FOR EROSION AND SEDIMENT CONTROL AND ENERGY DISSIPATION.
- CONSTRUCTION MATERIALS AND DEBRIS**  
CONSTRUCTION MATERIALS SHALL BE STORED IN AN ORDERLY MANNER AND IN AN AREA THAT WILL MINIMIZE CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES. CONSTRUCTION DEBRIS SHALL BE CONTAINED IN DUMPSTERS AND REMOVED FROM THE SITE AS NECESSARY.
- CHEMICALS**  
CHEMICALS SHALL BE STORED IN A SAFE AREA IN SEALED CONTAINERS WITH THE ORIGINAL LABELING AND MATERIAL SAFETY DATA SHEETS AVAILABLE.
- SPILLS AND CONTAMINATION**  
IF FUEL, OIL OR A HAZARDOUS CHEMICAL IS SPILLED OR DETECTED DURING CONSTRUCTION ACTIVITIES, ALL APPROPRIATE AGENCIES SHALL BE IMMEDIATELY NOTIFIED, INCLUDING, BUT NOT LIMITED TO, THE MINNESOTA DUTY OFFICER AT 800-422-0798.
- CONCRETE WASHOUT AREA**  
PERMITTEES MUST PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OPERATIONS (E.G., CONCRETE, STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE CONSTRUCTION ACTIVITY. PERMITTEES MUST PREVENT LIQUID AND SOLID WASHOUT WASTES FROM CONTACTING THE GROUND AND MUST DESIGN THE CONTAINMENT SO IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. PERMITTEES MUST PROPERLY DISPOSE LIQUID AND SOLID WASTES IN COMPLIANCE WITH MPCA RULES. PERMITTEES MUST INSTALL A SIGN INDICATING THE LOCATION OF THE WASHOUT FACILITY.

**POST CONSTRUCTION**  
 WHEN THE SITE HAS BEEN COMPLETELY CONSTRUCTED, THE SITE MUST UNDERGO FINAL STABILIZATION. FINAL STABILIZATION OCCURS WHEN ALL OF THE GRADING, INFRASTRUCTURE AND BUILDING ACTIVITIES HAVE BEEN COMPLETED. TO ACHIEVE FINAL STABILIZATION, THE FOLLOWING MEASURES SHALL BE COMPLETED.

- ALL DISTURBED AREAS WITHOUT PERMANENT IMPERVIOUS SURFACES SHALL BE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER. AREAS NOT REQUIRING SOD OR EROSION CONTROL BLANKET SHALL BE SEEDED AND MULCHED.
- SEDIMENT FROM CONVEYANCES AND TEMPORARY SEDIMENTATION BASINS THAT ARE TO BE USED AS PERMANENT WATER QUALITY MANAGEMENT BASINS SHALL BE CLEANED OUT. SEDIMENTATION BASINS SHALL BE SUFFICIENTLY CLEANED OUT TO RETURN THE BASIN TO DESIGN CAPACITY. SEDIMENT MUST BE STABILIZED TO PREVENT IT FROM BEING WASHED BACK INTO THE BASIN OR CONVEYANCES DISCHARGING OFF-SITE OR TO SURFACE WATERS.
- WHEN STABILIZED VEGETATION HAS BEEN ESTABLISHED OVER 70 PERCENT OF THE PEROVIOUS SURFACE AREA, ALL SYNTHETIC TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED. THIS INCLUDES, BUT IS NOT LIMITED TO, SILT FENCE, TREE FENCE AND CATCH BASIN INLET PROTECTION DEVICES.

**INSPECTIONS & RECORD KEEPING**  
 STORMWATER POLLUTION PREVENTION INSPECTIONS SHALL OCCUR EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS. INSPECTIONS MAY BE CEASED DURING FROZEN GROUND CONDITIONS. WHERE WORK HAS BEEN SUSPENDED DUE TO FROZEN GROUND CONDITIONS, THE REQUIRED INSPECTIONS AND MAINTENANCE MUST TAKE PLACE WITHIN 24 HOURS AFTER RUNOFF OCCURS AT THE SITE OR PRIOR TO RESUMING CONSTRUCTION, WHOEVER COMES FIRST. DURING THE COURSE OF CONSTRUCTION, IT MAY BE DETERMINED THAT ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES MAY BE NEEDED, OR CERTAIN MEASURES ARE NOT PRACTICAL TO INSTALL. IN THESE CASES, AN AMENDMENT TO THE SWPPP SHALL BE MADE, AND SUPPORTING REASONS SHALL BE DOCUMENTED IN THE SWPPP.

- THE EXCAVATOR IS RESPONSIBLE FOR ALL EROSION CONTROL INSPECTIONS.
- RECORD NAME OF INSPECTOR AND DATE AND TIME OF INSPECTION.
- RECORD RAINFALL AMOUNT SINCE MOST RECENT INSPECTION.
- INSPECT ROCK CONSTRUCTION ENTRANCES FOR SEDIMENTATION. INSPECT ADJACENT STREETS FOR SEDIMENT TRACKING.

- INSPECT SITE FOR EXCESSIVE EROSION AND SEDIMENT ACCUMULATION.
- INSPECT SILT FENCE AND OTHER TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES FOR EROSION, SEDIMENTATION AND MALFUNCTIONING.
- INSPECT FLARED END SECTIONS FOR EROSION AND SEDIMENTATION.
- INSPECT PONDS, INFILTRATION BASINS, TEMPORARY SEDIMENTATION BASINS AND ALL OTHER BMPs FOR EROSION AND SEDIMENTATION.
- INSPECT SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND CONVEYANCE SYSTEMS FOR EVIDENCE OF SEDIMENT BEING DEPOSITED BY EROSION.
- INSPECT STABILIZED AREAS FOR EROSION.
- RECORD RECOMMENDED REPAIRS, MAINTENANCE AND/OR REPLACEMENTS REQUIRED TO ENSURE EROSION AND SEDIMENTATION CONTROL MEASURES ARE SUFFICIENT.
- RECORD RECOMMENDED AMENDMENTS TO THE SWPPP.
- RECORD REPAIRS, MAINTENANCE AND/OR REPLACEMENTS THAT WERE COMPLETED SINCE THE LAST INSPECTION.

NOTE: FOR AREAS THAT HAVE UNDERGONE FINAL STABILIZATION, INSPECTIONS CAN BE REDUCED TO ONCE PER MONTH.

**MAINTENANCE**  
 THE OWNER/CONTRACTOR IS RESPONSIBLE FOR THE OPERATION, INSPECTION AND MAINTENANCE OF ALL STORMWATER POLLUTION PREVENTION MEASURES FOR THE DURATION OF THE PROJECT. THE FOLLOWING GUIDELINES SHALL BE USED TO DETERMINE NECESSARY REPAIRS, MAINTENANCE AND/OR REPLACEMENT OF THE EROSION AND SEDIMENTATION CONTROL MEASURES.

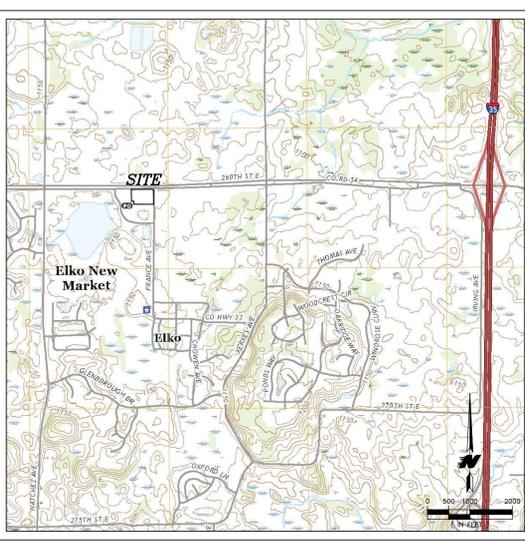
- ROCK CONSTRUCTION ENTRANCES SHALL BE REPAIRED OR REPLACED IF THE ROCK BECOMES INUNDATED WITH SEDIMENT AND/OR EXCESSIVE SEDIMENT IS BEING TRACKED FROM THE SITE. SEDIMENT TRACKED ONTO ADJACENT STREETS SHALL BE REMOVED. MEASURES SHALL BE TAKEN IMMEDIATELY UPON DISCOVERY.
- SILT FENCE SHALL BE REPAIRED OR REPLACED WHEN SEDIMENT REACHES 1/3 THE HEIGHT OF THE SILT FENCE. THE SILT FENCE IS DAMAGED AND/OR THE SILT FENCE BECOMES NONFUNCTIONAL. MEASURES SHALL BE TAKEN WITHIN 24 HOURS OF DISCOVERY.
- CATCH BASIN INLET PROTECTION DEVICES SHALL BE CLEANED WHEN SEDIMENT REACHES 1/3 THE HEIGHT OF THE SEDIMENT TRAP AND/OR REPAIRED OR REPLACED IF THE DEVICE BECOMES NONFUNCTIONAL. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.
- FLARED END SECTIONS SHALL BE CLEANED IF DEBRIS IS RESTRICTING FLOW OR IF SEDIMENT HAS ACCUMULATED AT THE OUTLET. IF A FLARED END SECTION BECOMES NONFUNCTIONAL OR DAMAGED, IT SHALL BE REPAIRED OR REPLACED. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.
- IF SEDIMENT IS OBSERVED OFF-SITE OR NEAR SURFACE WATERS, THE SOURCE OF SEDIMENT SHALL BE DETECTED AND ADDITIONAL MEASURES SHALL BE IMPLEMENTED. THE PERMITEE(S) SHALL COORDINATE SEDIMENT RETRIEVAL FROM SURFACE WATERS WITH ALL APPROPRIATE AGENCIES. MEASURES SHALL BE TAKEN WITHIN 7 DAYS OF DISCOVERY.
- PONDS, INFILTRATION BASINS, TEMPORARY SEDIMENTATION BASINS AND ALL OTHER BMPs SHALL BE CLEANED IF DEBRIS IS PRESENT AND/OR EXCESSIVE SEDIMENTATION HAS OCCURRED. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND THE SEDIMENT REMOVED WHEN SEDIMENT HAS FILLED THE BASIN TO 1/2 THE STORAGE VOLUME. NO SEDIMENT SHALL BE ALLOWED TO ACCUMULATE IN INFILTRATION BASINS. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.

**NOTICE OF TERMINATION**  
 THE PERMITEE(S) MUST SUBMIT A NOTICE OF TERMINATION (NOT) TO THE MPCA WITHIN 30 DAYS AFTER FINAL STABILIZATION IS COMPLETE, OR ANOTHER OWNER/OPERATOR (PERMITEE) HAS ASSUMED CONTROL OVER ALL AREAS OF THE SITE THAT HAVE NOT UNDERGONE FINAL STABILIZATION.

**QUANTITIES**  
 THE FOLLOWING TABLE PROVIDES ESTIMATED QUANTITIES FOR STORMWATER POLLUTION PREVENTION THROUGHOUT THE PROJECT.

ITEM	UNIT	ESTIMATED QUANTITY
ROCK ENTRANCE	EA.	1
SILT FENCE	L.F.	1,150
INLET PROTECTION	EA.	10
TURF ESTABLISHMENT	AC.	1.5

**SITE LOCATION MAP**



**GENERAL INFORMATION**  
 MINNESOTA'S CONSTRUCTION STORMWATER PERMIT IS AN EXTENSION OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM STORMWATER PROGRAM, WHICH IS PART OF THE FEDERAL CLEAN WATER ACT. REGULATED PARTIES MUST DEVELOP A STORMWATER POLLUTION PREVENTION PLAN (SWPPP). THE SWPPP PROVIDES INFORMATION ON THE EXISTING AND PROPOSED SITE CONDITIONS, CONTROL MEASURES FOR STORMWATER POLLUTION PREVENTION BEFORE, DURING AND AFTER CONSTRUCTION, INSPECTION, MAINTENANCE AND INFORMATION RELATED TO THE PERMANENT STORMWATER MANAGEMENT SYSTEM. THE SWPPP SHALL BE KEPT ON SITE AT ALL TIMES DURING ACTIVE CONSTRUCTION.

**PROJECT INFORMATION**  
 PROJECT NAME: CONVENIENCE STORE 1116 WITH SINGLE BAY CARWASH & SIDE DIESEL  
 PROJECT LOCATION: ELKO NEW MARKET, SCOTT COUNTY, MINNESOTA  
 PROJECT OWNER: KWIK TRIP, INC.

**RESPONSIBLE PARTIES**  
 THE OWNER MUST IDENTIFY A PERSON KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMPs WHO WILL OVERSEE THE IMPLEMENTATION OF THE SWPPP, AND THE INSTALLATION, INSPECTION AND MAINTENANCE OF THE EROSION PREVENTION AND SEDIMENT CONTROL BMPs.

SITE MANAGER: STEVE LOWE - KWIK TRIP INC.

TRAINING DOCUMENTATION: CONSTRUCTION SITE MANAGEMENT (5/31/20 EXPIRATION) - UNIVERSITY OF MN

**EXISTING SITE CONDITIONS**  
 THE PROPOSED KWIK TRIP SITE IS LOCATED IN THE SOUTHWEST QUADRANT OF THE INTERSECTION OF COUNTY ROAD 2 AND FRANCE AVENUE IN ELKO NEW MARKET, SCOTT COUNTY, MINNESOTA. THE SITE IS BOUNDED ON THE NORTH BY COUNTY ROAD 2, ON THE EAST BY FRANCE AVENUE, ON THE SOUTH BY UNDEVELOPED COMMERCIAL PROPERTY, AND ON THE WEST BY UNDEVELOPED COMMERCIAL PROPERTY AND A UNITED STATES POSTAL SERVICE BUILDING.

THE PROPOSED KWIK TRIP SITE CONSISTS OF OUTLOT A OF THE ELKO MARKET PLACE 1ST ADDITION PLAT AND CONTAINS 4.08 ACRES. THERE IS APPROXIMATELY 0.08 ACRES OF EXISTING IMPERVIOUS SURFACE, WHICH CONSISTS OF A PORTION OF THE POSTAL SERVICE PARKING LOT AND A SMALL AREA IN THE NORTHEAST CORNER OF THE SITE.

**PROPOSED SITE CONDITIONS**  
 KWIK TRIP, INC. PLANS ON DEVELOPING THE SITE INTO A CONVENIENCE STORE WITH SINGLE BAY CARWASH, TWO FUELING CANOPIES, AND ASSOCIATED PARKING AND DRIVE AREAS. DURING CONSTRUCTION OF THE KWIK TRIP SITE, APPROXIMATELY 3.5 ACRES WILL BE DISTURBED. AFTER CONSTRUCTION, THE KWIK TRIP LOT WILL CONTAIN APPROXIMATELY 2.34 ACRES OF IMPERVIOUS SURFACE.

STORM SEWER WILL BE USED TO COLLECT STORMWATER FROM THE KWIK TRIP SITE. THE STORM SEWER WILL BE CONNECTED TO TRUNK STORM SEWER THAT WAS STUBBED TO THE SITE AS PART OF THE OVERALL ELKO MARKET PLACE DEVELOPMENT. ALL STORMWATER FROM THE DEVELOPED PORTIONS OF THE SITE, EXCEPT FOR SOME PEROVIOUS AREAS ALONG FRANCE AVENUE AND NORTH OF THE PROPOSED STATION STORE, WILL DRAIN TO TRUNK STORM SEWER THAT DISCHARGES TO A REGIONAL POND SOUTH OF THE SITE FOR WATER QUALITY TREATMENT AND RATE CONTROL.

**SOIL INFORMATION**  
 IN DECEMBER OF 2019, BRAUN INTERTEC DRILLED FIVE SOIL BORINGS TO APPROXIMATE NEAR SURFACE SOILS. THE BORINGS INDICATE THAT NEAR SURFACE SOILS CONSIST PRIMARILY OF LEAN CLAY AND SANDY CLAY. THESE SOILS GENERALLY FALL WITHIN THE HYDROLOGIC SOIL GROUP (HSG) "D".

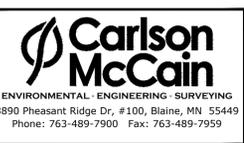
**WETLAND CONSIDERATIONS**  
 THERE ARE NO KNOWN WETLANDS PRESENT ON THE SITE.

**STORMWATER RECEIVING WATERS**  
 STORMWATER FROM THE SITE DISCHARGES SOUTH VIA STORM SEWER TO A REGIONAL STORMWATER POND SOUTH OF THE SITE.

**SPECIAL/IMPAIRED WATER CONSIDERATIONS**  
 THE VERMILION RIVER IS LOCATED APPROXIMATELY 0.6 MILES NORTH OF THE SITE IS AN IMPAIRED WATER. ADDITIONAL BMPs INCLUDE: IMMEDIATE STABILIZATION OF EXPOSED SOIL AREAS, AND COMPLETE STABILIZATION WITHIN SEVEN (7) CALENDAR DAYS AFTER CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE TEMPORARILY OR PERMANENTLY CEASES, AND TEMPORARY SEDIMENTATION BASINS FOR COMMON DRAINAGE AREAS OF FIVE (5) ACRES OR MORE.



**KWIK TRIP, Inc.**  
 P.O. BOX 2107  
 1626 OAK STREET  
 LA CROSSE, WI 54602-2107  
 PH. (608) 781-8988  
 FAX (608) 781-8960



I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Name: Joseph T. Radach, P.E.  
 Signature: [Signature]  
 Date: 05/15/20 License #: 45889

**STORMWATER POLLUTION PREVENTION PLAN**

**CONVENIENCE STORE #1116 WITH SINGLE BAY CARWASH AND SIDE DIESEL**

**260TH ST E & FRANCE AVE ELKO NEW MARKET, MINNESOTA**

#	DATE	DESCRIPTION

DRAWN BY: JTR  
 SCALE: GRAPHIC  
 PROJ. NO.: 8622-00  
 DATE: 2020-05-15  
 SHEET: 1116 SP5

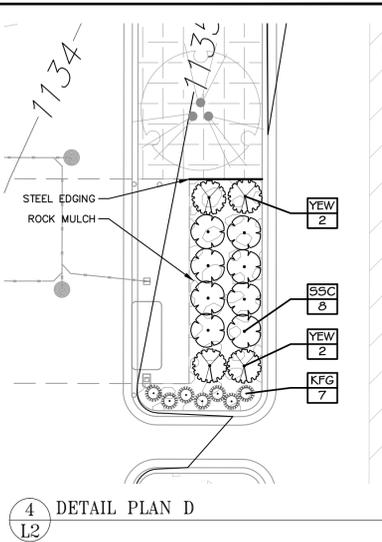
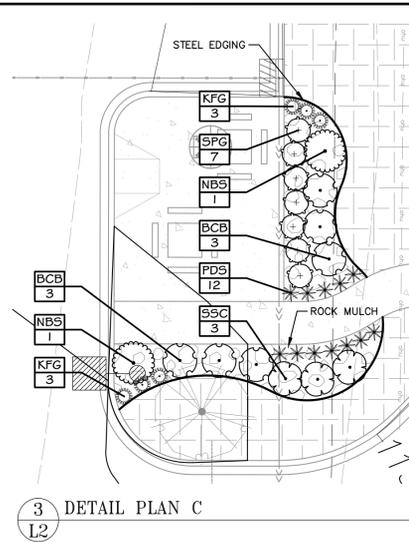
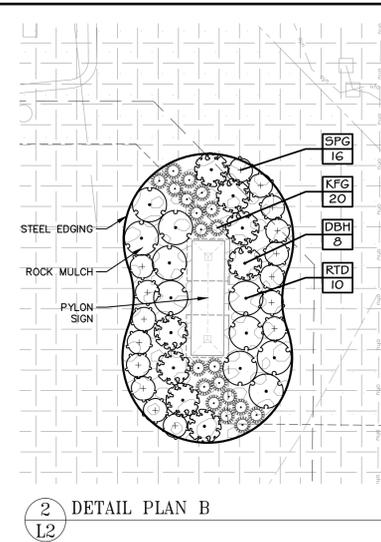
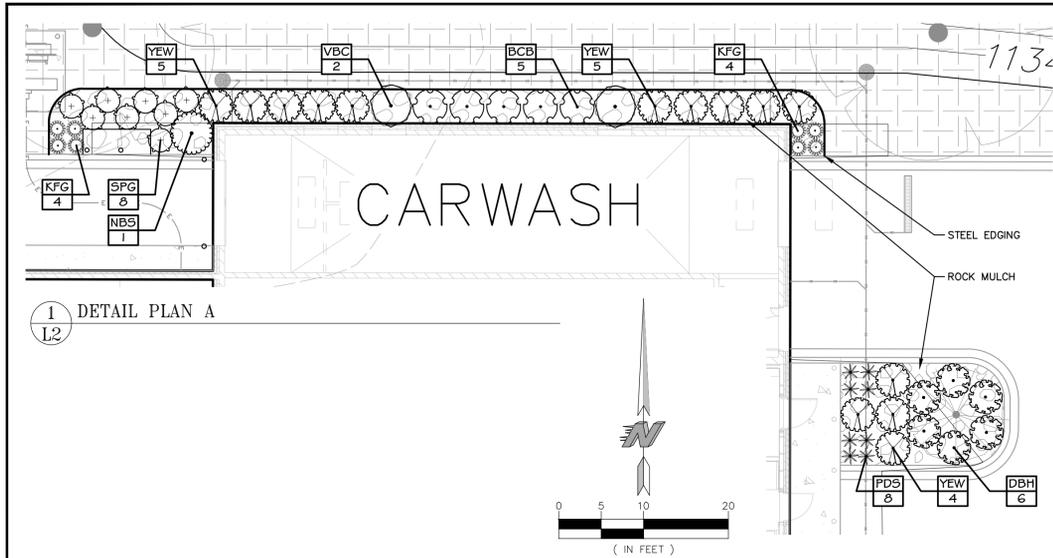




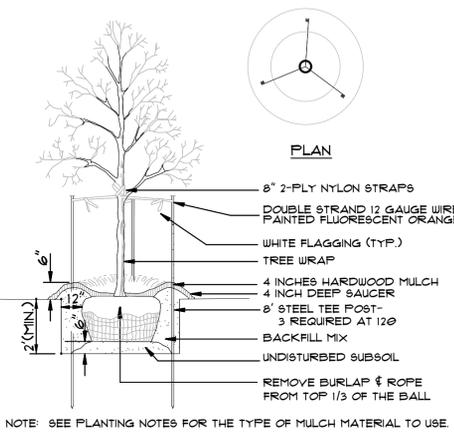




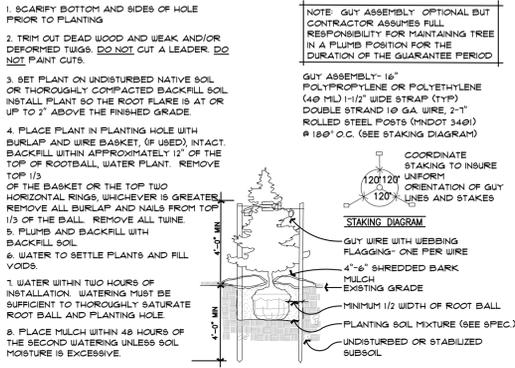




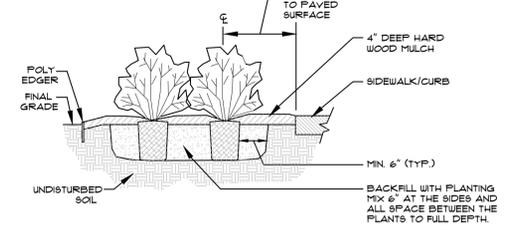
PLANT SCHEDULE						
TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	FM	3	Acer x freemanii 'Sienna'	Sienna Glen Maple	2.5' Cal.	B&B
	HL	3	Gleditsia triacanthos inermis 'Harve'	Northern Acclaim Thornless Honey Locust	2.5' Cal.	B&B
	WO	4	Quercus bicolor	Swamp White Oak	2.5' Cal.	B&B
	BL	3	Tilia americana 'Boulevard'	Boulevard Linden	2.5' Cal.	B&B
CONIFEROUS TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	BF	3	Abies balsamea	Balsam Fir	6' Ht.	B&B
	BH	6	Picea glauca densata	Black Hills Spruce	6' Ht.	B&B
ORNAMENTAL TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	SB	7	Amelanchier x grandiflora 'Autumn Brilliance' Clump Form, 1.5' Cal Equivalent	Autumn Brilliance Serviceberry	8' Ht.	B&B
	JL	3	Syringa reticulata 'Ivory Silk' White Flowers	Ivory Silk Japanese Tree Lilac	1.5' Cal.	B&B
SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	BCB	11	Aronia melanocarpa 'Autumn Magic'	Autumn Magic Black Chokeberry	#5 Cont.	
	SSC	11	Clethra alnifolia 'Hummingbird'	Summersweet	#5 Cont.	
	RTD	10	Cornus sericea 'Alleman's Compact'	Dwarf Red Twig Dogwood	#5 Cont.	
	DBH	14	Diervilla lonicera	Dwarf Bush Honeysuckle	#5 Cont.	
	NBS	3	Physocarpus opulifolius 'Seward' TM	Summer Wine Seward Ninebark	#5 Cont.	
	SPG	31	Spiraea x bumalda 'Goldflame' Gold Foliage, Red Flowers	Goldflame Spirea	#5 Cont.	
	VBC	2	Viburnum trilobum 'Bailey Compact' Red Fall Color	Bailey's Compact American Cranberry Bush	#5 Cont.	
EVERGREEN SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	YEW	18	Taxus x media 'Tautonii'	Tauton Yew	#5 Cont.	
GRASSES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	KFG	41	Calamagrostis x acutiflora 'Karl Foerster'	Feather Reed Grass	#5 Cont.	
	PDS	20	Sporobolus heterolepis	Prairie Dropseed	#5 Cont.	
GROUND COVERS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	ROCK	1,769 sf	Rock Mulch Non-Woven Geotextile Incidental	1.5" Trap Rock Mulch	4" Depth	
	SOD	61,457 sf	Turf Sod Bluegrass	Kentucky Bluegrass	sod	



**A DECIDUOUS TREE PLANTING DETAIL**  
NOT TO SCALE



**B CONIFEROUS TREE PLANTING DETAIL**  
NOT TO SCALE



**C SHRUB & CONTAINER PLANTING DETAIL**  
NOT TO SCALE

**LANDSCAPE SPECIFICATIONS**

- TREE PROTECTION.** ALL TREES NOT SPECIFICALLY NOTED OR MARKED ON SITE FOR REMOVAL SHALL REMAIN PROTECTED AND UNDISTURBED DURING CONSTRUCTION. TREE PROTECTION SHALL EXTEND TO THE DRIP LINE, WITHIN WHICH NO CONSTRUCTION ACTIVITY, MATERIAL STORAGE, OR VEHICLE PARKING SHALL BE PERMITTED. TREE PROTECTION FENCING SHALL BE ERECTED PRIOR TO CONSTRUCTION START PER PLANS OR AS DIRECTED BY OWNER/LANDSCAPE ARCHITECT AND SHALL CONSIST OF 4' TALL HEAVY DUTY ORANGE CONSTRUCTION FENCING WITH 6\"/>
- EROSION CONTROL.** REFER TO CIVIL PLAN SHEETS FOR STORMWATER POLLUTION PREVENTION PLAN (SWPPP), AND TEMPORARY AND PERMANENT STORMWATER BMPs, INCLUDING SILT FENCE, BIO-ROLLS, INLET PROTECTION, EROSION CONTROL BLANKETING, DUST CONTROL, SWEEPING AND ROCK CONSTRUCTION ENTRANCE. ALL DISTURBED AREAS SHALL RECEIVE PERMANENT STABILIZATION IN ACCORDANCE WITH THE LANDSCAPE PLAN WITHIN 7 DAYS AFTER CONSTRUCTION ACTIVITY IN THE DISTURBED AREA HAS CEASED. IN THE EVENT PERMANENT STABILIZATION CANNOT BE IMPLEMENTED WITHIN 7 DAYS, TEMPORARY STABILIZATION BMPs MUST BE IMPLEMENTED WITHIN 7 DAYS USING.
- CLEARING AND GRUBBING.** CONTRACTOR SHALL BE RESPONSIBLE FOR CLEARING AND GRUBBING ALL AREAS INDICATED AS BEING DISTURBED OR OTHERWISE SHOWN ON PLANS. CLEARING AND GRUBBING SHALL INCLUDE REMOVAL AND DISPOSAL OF ALL TREES, STUMPS, BRUSH, GRASS, STUMPS AND OTHER ORGANIC MATERIAL AT AN APPROVED OFF-SITE DISPOSAL LOCATION.
- SOIL PREPARATION.** REFER TO GEOTECHNICAL REPORT FOR ANY REQUIRED SOIL CORRECTIONS, AMENDMENTS OR ADDITIONAL INFORMATION (IF APPLICABLE). EXISTING TOPSOIL SHALL BE STRIPPED FROM ALL DISTURBED AREAS AND STOCKPILED IN AN APPROVED LOCATION FOR RE-SPREAD. ALL AREAS WHERE SOIL HAS BEEN COMPACTED BY CONSTRUCTION ACTIVITY AND THAT ARE INDICATED TO BE SODDED, SEEDED OR PLANTING BED SHALL BE DE-COMPACTED TO A MINIMUM DEPTH OF 12 INCHES BY SOIL RIPPING, TILLING OR OTHER APPROVED SOIL LOOSENING METHOD.
- TOPSOIL MATERIAL.** ALL EXISTING, AMENDED OR IMPORTED TOPSOIL SHALL MEET THE REQUIREMENTS OF MNDOT TOPSOIL TYPE A. A MINIMUM 6 INCH DEPTH OF TOPSOIL SHALL BE PLACED ON ALL AREAS TO BE SODDED OR SEED. A MINIMUM 12 INCH DEPTH OF TOPSOIL SHALL BE PLACED WITHIN ALL PLANTING BED AREAS. ALL TOPSOIL SHALL BE FINE GRADED, RAKED AND DRAGGED TO PROVIDE A SMOOTH, UNIFORM SURFACE. TOPSOIL GRADES SHALL BE WITHIN 1 FOOT OF INDICATED FINISHED GRADE AND SHALL BE TRUE TO GRADIENTS SHOWN ON PLANS. REFER TO CIVIL PLAN SHEETS FOR FILTRATION BASIN SOIL REQUIREMENTS.
- SEEDING AND TURF ESTABLISHMENT.** CONTRACTOR SHALL OBTAIN OWNER/LANDSCAPE ARCHITECT'S APPROVAL OF FINAL GRADES AND TOPSOIL PREP PRIOR TO SEEDING. APPLY 12-12-12 GRANULAR STARTER FERTILIZER AT A RATE OF 250 LBS PER ACRE PRIOR TO SEEDING. SEEDS SHALL BE SOWN IN 2 PERPENDICULAR PASSES, EACH PASS AT ONE-HALF THE INDICATED RATE, VIA BROADCAST SPREADER, DROP SEEDER OR DRILL SEEDER. FOLLOWING SEED APPLICATION, INSTALL TYPE 3N EROSION CONTROL BLANKET ON ALL SLOPES GREATER THAN 4:1. IN ALL OTHER AREAS, APPLY HYDROMULCH COVER (MUST BE A SEPARATE OPERATION FROM SEEDING) AT A TARGETED DRY WEIGHT RATE OF 3500 LBS PER ACRE. SOIL SHALL BE KEPT MOIST DURING ESTABLISHMENT WITH ADDITIONAL RE-SEEDING AS NECESSARY TO ACHIEVE A HEALTHY, UNIFORM STAND OF GRASS, FREE OF WEEDS AND WITH COVER EXCEEDING 75% IN ANY 10'x10' AREA PRIOR TO FINAL ACCEPTANCE.
- SODDING.** CONTRACTOR SHALL OBTAIN OWNER/LANDSCAPE ARCHITECT'S APPROVAL OF FINAL GRADES AND TOPSOIL PREP PRIOR TO SODDING. APPLY 12-12-12 GRANULAR STARTER FERTILIZER AT A RATE OF 250 LBS PER ACRE PRIOR TO SODDING AND ROLL TOPSOIL TO CREATE A UNIFORM SURFACE FOR LAYING SOD. SOD SHALL NOT BE CUT MORE THAN 24-HOURS IN ADVANCE OF INSTALLATION. CONTRACTOR SHALL KEEP SOD MOIST FOR A MINIMUM OF 30 DAYS AND SHALL BE RESPONSIBLE FOR MAINTAINING THE SOD UNTIL FINAL ACCEPTANCE.
- PLANT MATERIAL.** ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-Z60, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERYMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS. OWNER/LANDSCAPE ARCHITECT RESERVE THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION. NO SUBSTITUTION OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE OWNER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- PLANT MATERIAL SUBSTITUTIONS.** ALL REQUESTS FOR PLANT SUBSTITUTIONS SHALL BE MADE IN WRITING TO THE OWNER/LANDSCAPE ARCHITECT AND MUST BE APPROVED BY THE CITY.
- PLANT INSTALLATION AND ESTABLISHMENT.** REFER TO STANDARD PLANTING DETAILS. CONTRACTOR SHALL STAKE TREE LOCATIONS FOR APPROVAL BY OWNER/LANDSCAPE ARCHITECT PRIOR TO PLANTING. ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE.
- MULCH MATERIAL.** DOUBLE SHREDDED HARDWOOD MULCH OR ROCK MULCH AS INDICATED ON PLANS. ALL MULCH SHALL BE CLEAN AND FREE OF NOXIOUS WEEDS, SOIL, OR OTHER DELETERIOUS MATERIAL, AND SHALL BE INSTALLED OVER A NON-WOVEN GEOTEXTILE FABRIC (INCIDENTAL) OR OTHER APPROVED WEED BARRIER TO A MINIMUM SETTLED DEPTH OF 4\"/>
- LANDSCAPE EDGING.** INSTALL LANDSCAPE EDGING BETWEEN ALL MULCH AREAS AND TURF. EDGING SHALL BE COMMERCIAL GRADE BLACK POLYETHYLENE OR VINYL EDGING, 0.1 INCH THICK BY 5 INCHES DEEP, V-LIPPED BOTTOM, HORIZONTALLY GROOVED, 1-INCH ROUND TOP, EXTRUDED IN STANDARD LENGTHS, WITH 9-INCH STEEL ANGLE STAKES.
- IRRIGATION.** DESIGN, FURNISH AND INSTALL A COMPLETE UNDERGROUND IRRIGATION SYSTEM FROM APPROVED POINT(S)-OF-CONNECTION WITHIN THE SITE COVERING ALL TURF AND PLANTING AREAS AS SHOWN ON THE LANDSCAPE PLAN. INCLUDES FLOW/PRESSURE TESTING, PLANS WITH DESIGN CALCULATIONS, AS-BUILT DRAWINGS, LABOR, MATERIALS, EQUIPMENT, AND SERVICES FOR THE TESTING, ADJUSTING, RETESTING AND READJUSTING AS REQUIRED TO PLACE THE SYSTEM IN AN APPROVED OPERATING CONDITION. THE IRRIGATION SYSTEM SHALL INCLUDE THE DESIGN AND INSTALLATION OF THE FOLLOWING: PIPING, METER AND BACKFLOW ASSEMBLIES, SPRINKLER HEADS, CABINETS, VALVES AND VALVE BOXES, CONTROLLERS, CONTROL WIRING, FITTINGS, ELECTRICAL CONNECTIONS, QUICK-COUPERS, ALL OTHER NECESSARY ACCESSORIES, SYSTEM MANUALS, 1-YEAR MAINTENANCE PERIOD INCLUDING 1 FALL WINTERIZATION AND 1 SPRING START-UP. IRRIGATION PLANS TO BE PREPARED BY A QUALIFIED IRRIGATION DESIGNER AND SUBMITTED TO OWNER/LANDSCAPE ARCHITECT FOR APPROVAL.
- MAINTENANCE.** MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE, INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE MOWING, TRIMMING, WATERING, FERTILIZING, WEED AND PESTICIDE CONTROL, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL RETAIN RESPONSIBILITY FOR ALL PLANT MATERIAL THROUGH THE COMPLETION OF THE WARRANTY PERIOD.
- WATERING.** UPON ESTABLISHMENT OF SEED AND INSTALLATION OF PLANTS, CONTRACTOR SHALL MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS AND TURF AREAS A MINIMUM OF ONCE A WEEK. MORE FREQUENT WATERING MAY BE REQUIRED DURING PERIODS OF HOT, DRY WEATHER. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER. IN THE ABSENCE OF PERMANENT IRRIGATION, TEMPORARY IRRIGATION, TREE WATERING BAGS, OR HAND-WATERING ARE ACCEPTABLE.
- FINAL ACCEPTANCE.** UPON SUBSTANTIAL COMPLETION OF THE WORK, CONTRACTOR SHALL REQUEST FINAL ACCEPTANCE OF THE WORK IN WRITING BY THE OWNER/LANDSCAPE ARCHITECT. IF ANY WORK IS FOUND TO BE INCOMPLETE OR UNSATISFACTORY IN THE OPINION OF THE OWNER/LANDSCAPE ARCHITECT, A WRITTEN PUNCH LIST WILL BE PREPARED LISTING ALL ITEMS THAT REQUIRE COMPLETING OR CORRECTING BEFORE FINAL ACCEPTANCE.
- WARRANTY.** ALL PLANTS, MATERIALS AND WORKMANSHIP SHALL BE GUARANTEED FOR ONE YEAR FROM THE DATE OF FINAL ACCEPTANCE, UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND MATERIAL.



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I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.  
Name: Ryan J. B...  
Signature: [Signature]  
Date: 05/19/20 License #: 56346

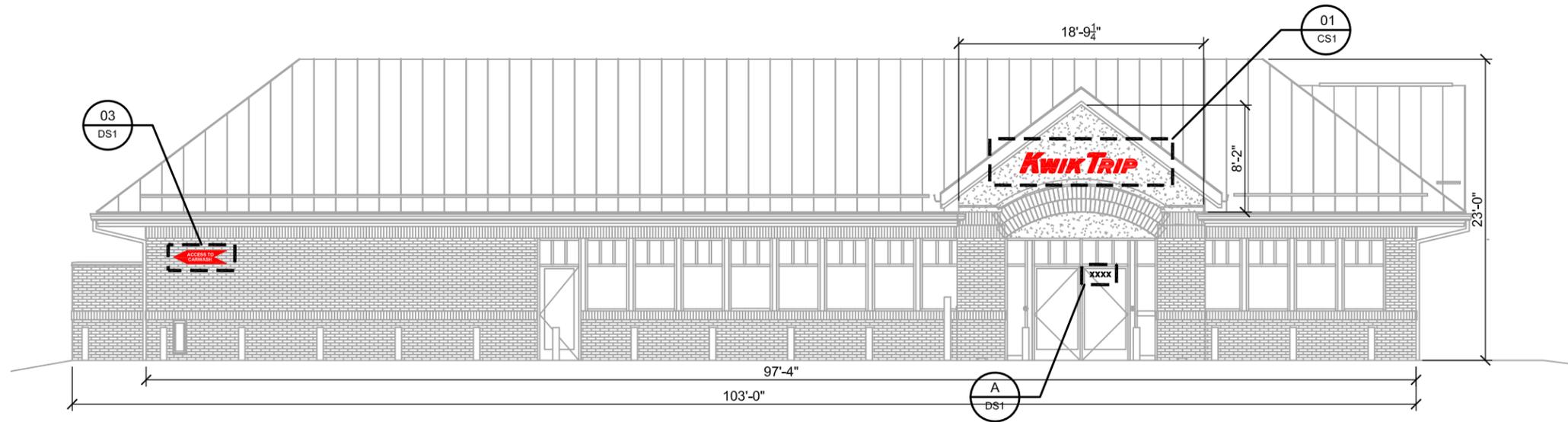
LANDSCAPE PLAN  
CONVENIENCE STORE #1116  
WITH SINGLE BAY CARWASH  
AND SIDE DIESEL  
260TH ST E & FRANCE AVE  
ELKO NEW MARKET, MINNESOTA

#	DATE	DESCRIPTION

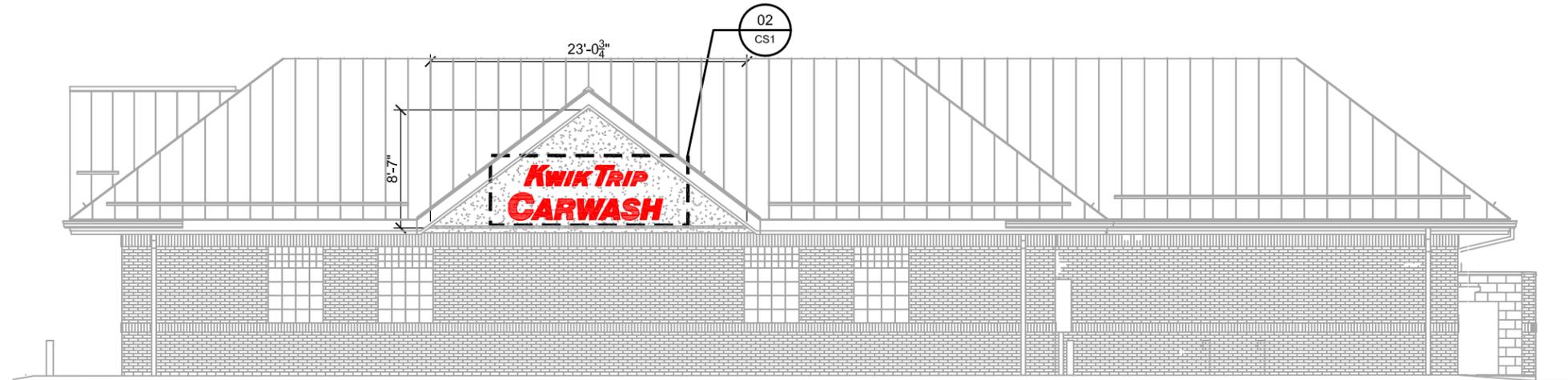
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SCALE: GRAPHIC  
PROJ. NO.: 8622-00  
DATE: 2020-05-15  
SHEET: 1116 L2



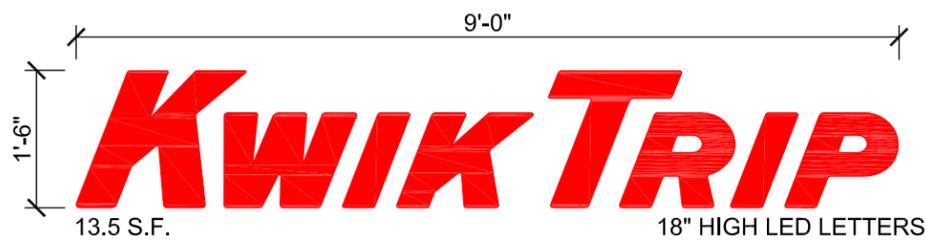




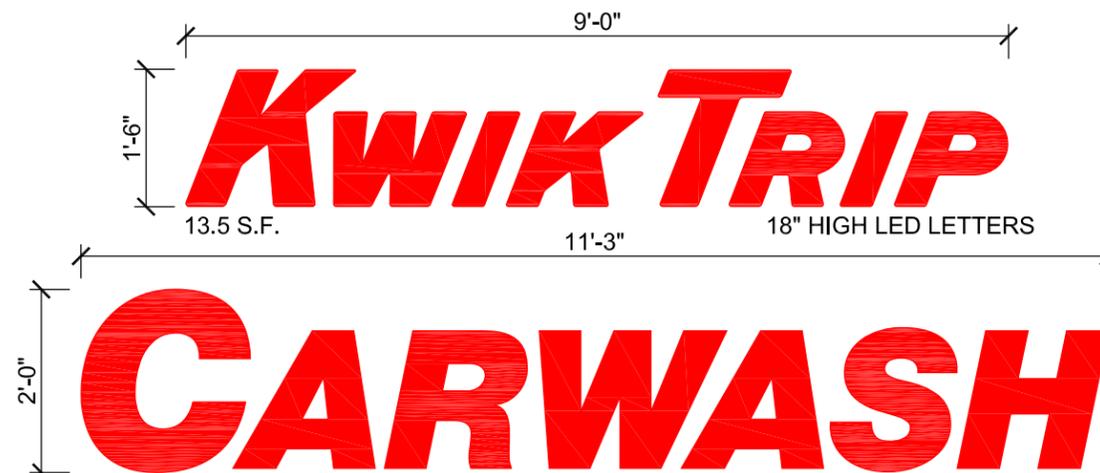
**STORE ELEVATION**  
SCALE: 3/32" = 1'-0"



**STORE ELEVATION**  
SCALE: 3/32" = 1'-0"



**LOGO DETAIL - SIGN #01**  
SCALE: 1/2" = 1'-0"



**LOGO DETAIL - SIGN #02**  
SCALE: 1/2" = 1'-0"

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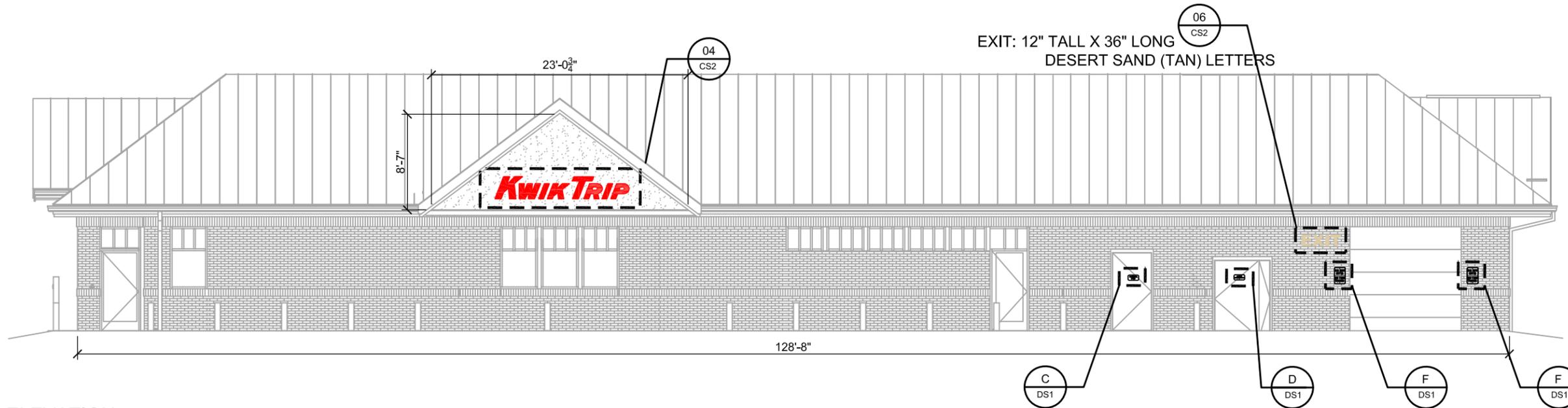
CONVENIENCE STORE SIGNAGE

CONVENIENCE STORE #1116  
w/ SIDE DIESEL & SINGLE BAY CW

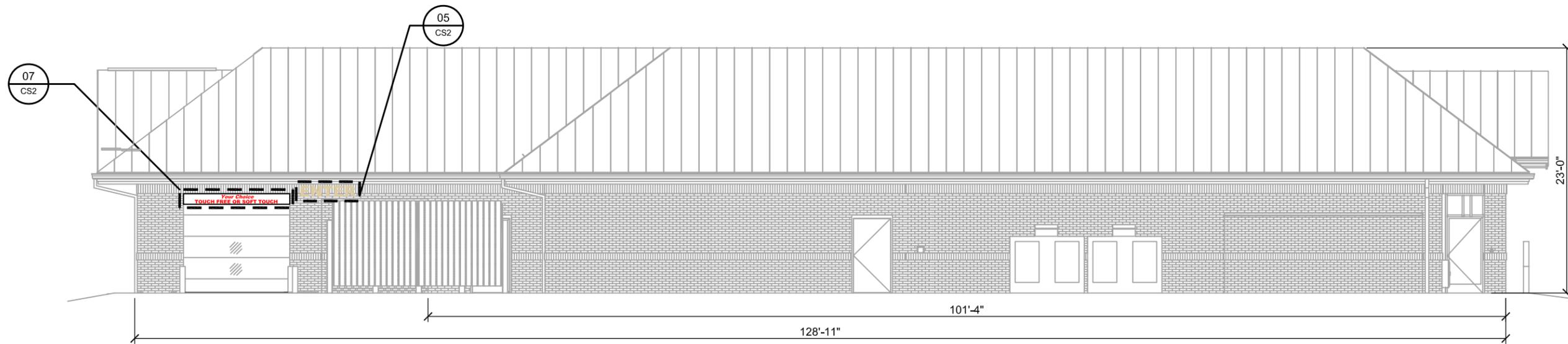
260TH ST E & FRANCE AVE  
ELKO, MN

#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

DRAWN BY	KMK
SCALE	MULTIPLE
PROJ. NO.	0001
DATE	2020-03-25
SHEET	CS1



**STORE ELEVATION**  
SCALE: 3/32" = 1'-0"



**STORE ELEVATION**  
SCALE: 3/32" = 1'-0"



**LOGO DETAIL - SIGN #04**  
SCALE: 1/2" = 1'-0"

***Your Choice***  
**TOUCH FREE OR SOFT TOUCH**

NON-LIT INFORMATIONAL SIGN  
RED VINYL ON WHITE ALUMINUM  
1'-0"H X 10'-0"W = 10.0 SQ FT

**INFORMATIONAL SIGN #07**  
SCALE: 1/2" = 1'-0"

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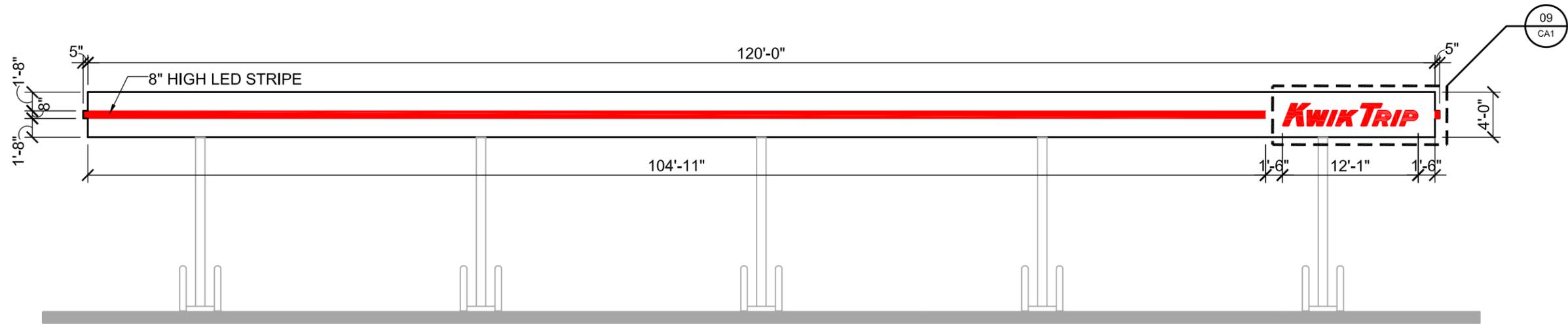
CONVENIENCE STORE SIGNAGE

CONVENIENCE STORE #1116  
w/ SIDE DIESEL & SINGLE BAY CW

260TH ST E & FRANCE AVE  
ELKO, MN

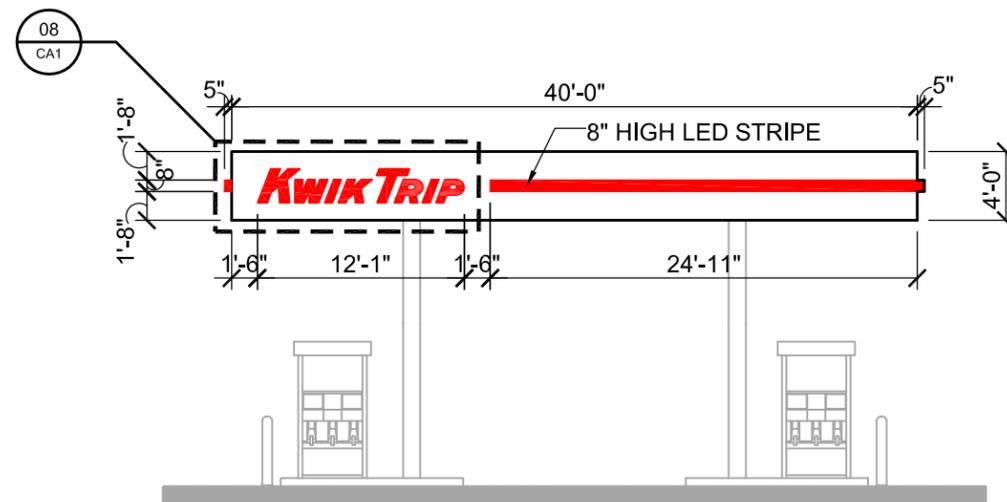
#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

DRAWN BY	KMK
SCALE	MULTIPLE
PROJ. NO.	0001
DATE	2020-03-25
SHEET	CS2



**CANOPY ELEVATION**

SCALE: 3/32" = 1'-0"



**CANOPY ELEVATION**

SCALE: 3/32" = 1'-0"



**LOGO DETAIL - SIGNS #08 & #09**

SCALE: 1/2" = 1'-0"

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**Kwik  
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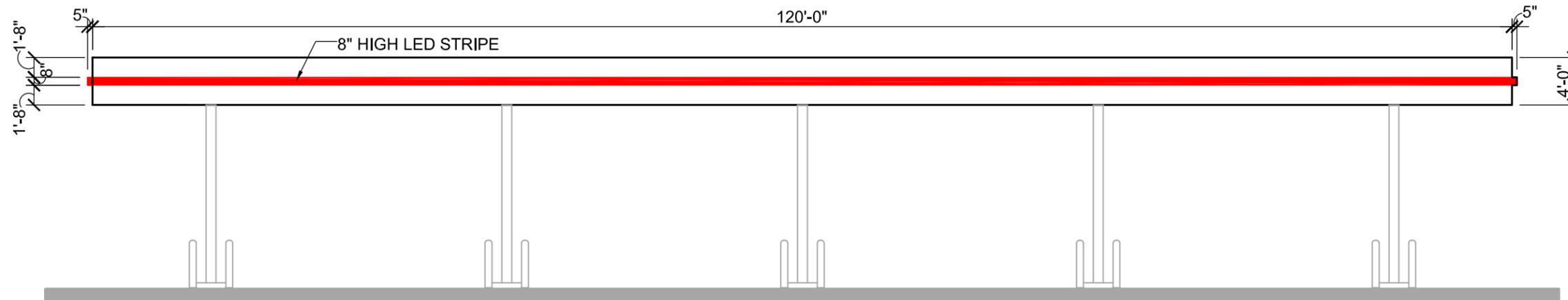
CANOPY SIGNAGE

CONVENIENCE STORE #1116  
w/ SIDE DIESEL & SINGLE BAY CW

260TH ST E & FRANCE AVE  
ELKO, MN

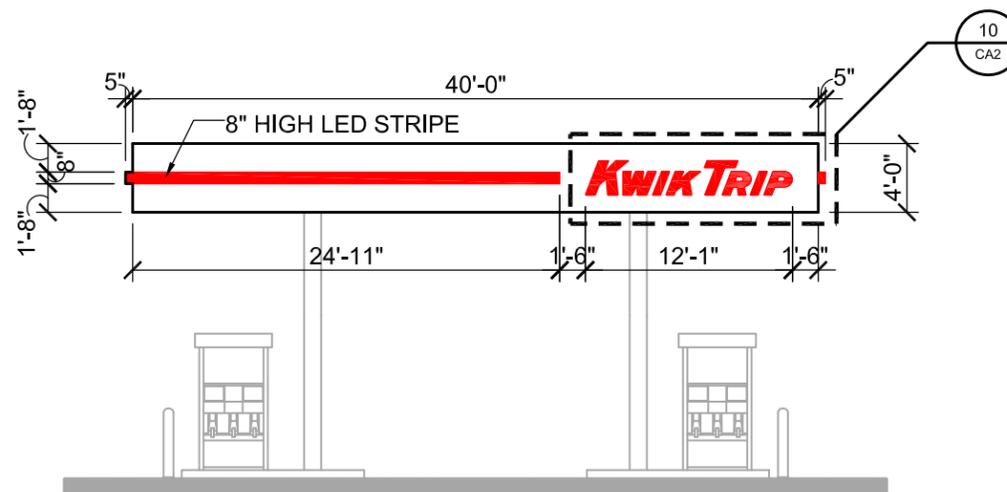
#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

DRAWN BY	KMK
SCALE	MULTIPLE
PROJ. NO.	0001
DATE	2020-03-25
SHEET	CA1



**CANOPY ELEVATION**

SCALE: 3/32" = 1'-0"



**CANOPY ELEVATION**

SCALE: 3/32" = 1'-0"



**LOGO DETAIL - SIGN #10**

SCALE: 1/2" = 1'-0"

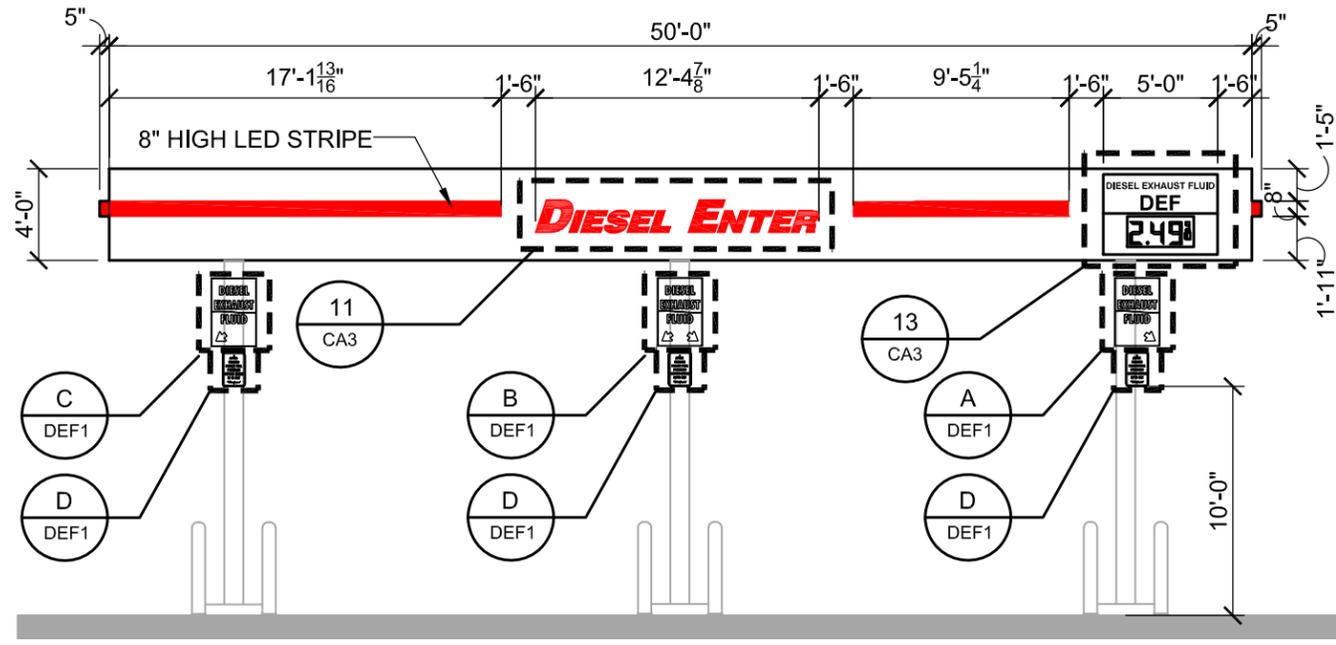


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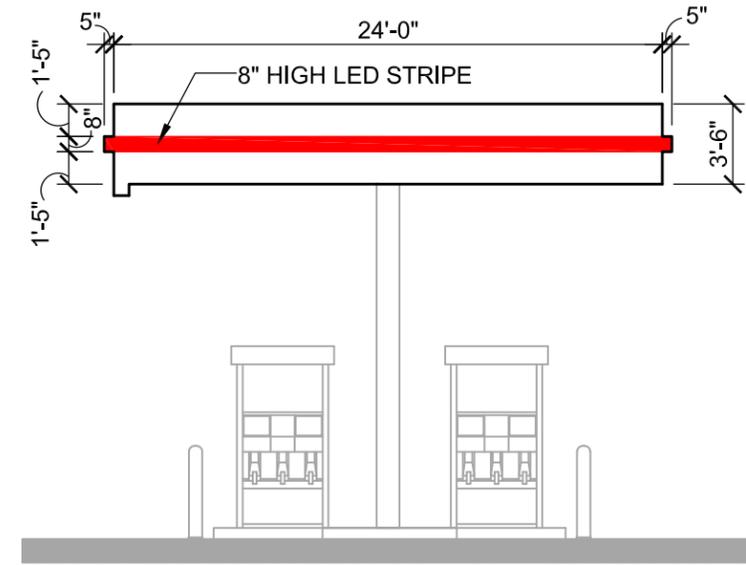
CANOPY SIGNAGE  
CONVENIENCE STORE #1116  
w/ SIDE DIESEL & SINGLE BAY CW  
260TH ST E & FRANCE AVE  
ELKO, MN

#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

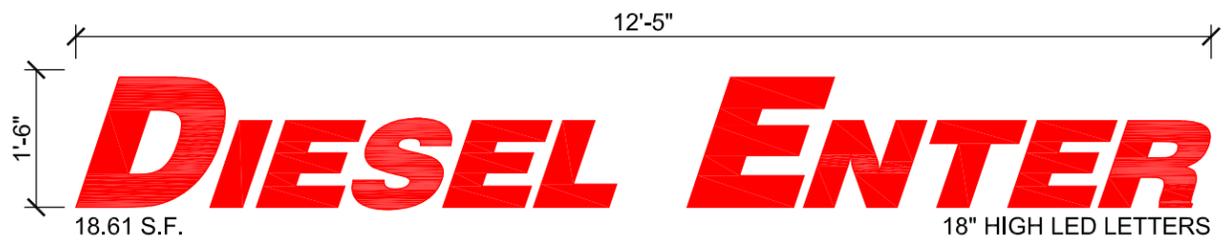
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PROJ. NO.: 0001  
DATE: 2020-03-25  
SHEET: CA2



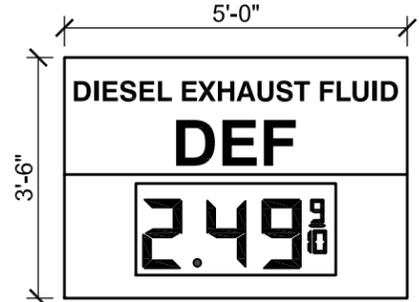
**CANOPY ELEVATION**  
SCALE: 1/8" = 1'-0"



**CANOPY ELEVATION**  
SCALE: 1/8" = 1'-0"



**LOGO DETAIL - SIGN #11**  
SCALE: 1/2" = 1'-0"

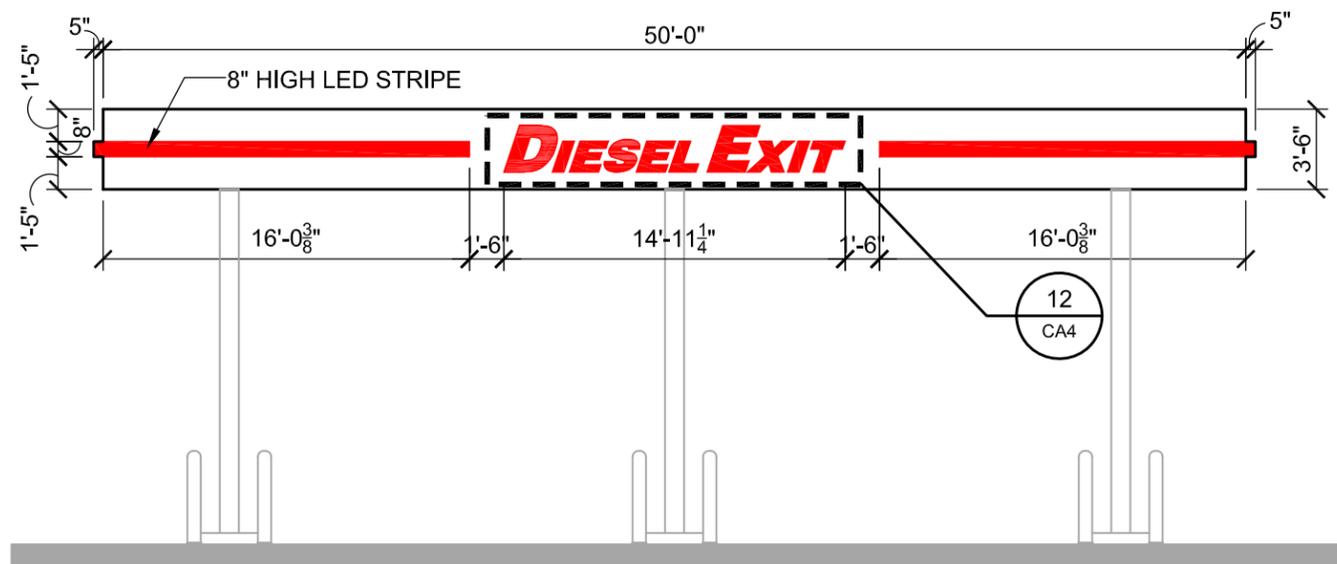


**DEF DETAIL - SIGN #13**  
SCALE: 3/8" = 1'-0"



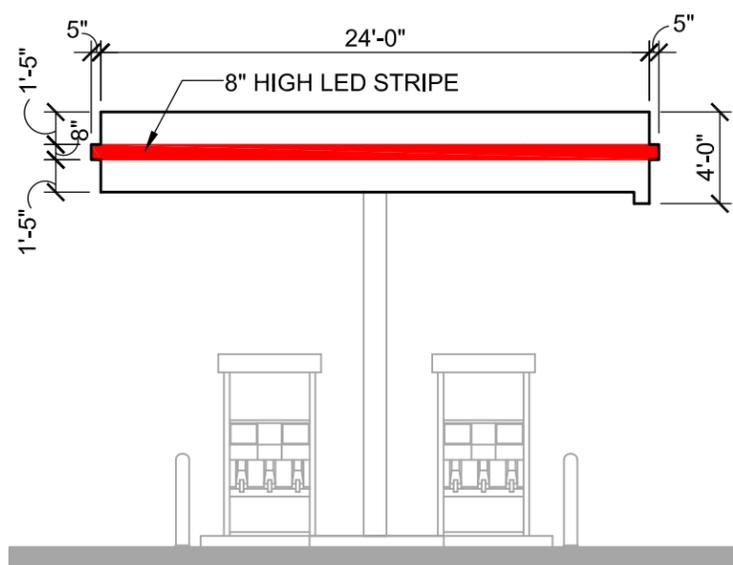
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CANOPY SIGNAGE	CONVENIENCE STORE #11116	
	w/ SIDE DIESEL & SINGLE BAY CW	
	260TH ST E & FRANCE AVE ELKO, MN	
#	DATE	DESCRIPTION
△	2020/05/18	BKGD UPDATE
DRAWN BY	KMK	
SCALE	MULTIPLE	
PROJ. NO.	0001	
DATE	2020-03-25	
SHEET	CA3	



**CANOPY ELEVATION**

SCALE: 1/8" = 1'-0"



**CANOPY ELEVATION**

SCALE: 1/8" = 1'-0"



**LOGO DETAIL - SIGN #12**

SCALE: 1/2" = 1'-0"

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CANOPY SIGNAGE

CONVENIENCE STORE #1116  
w/ SIDE DIESEL & SINGLE BAY CW

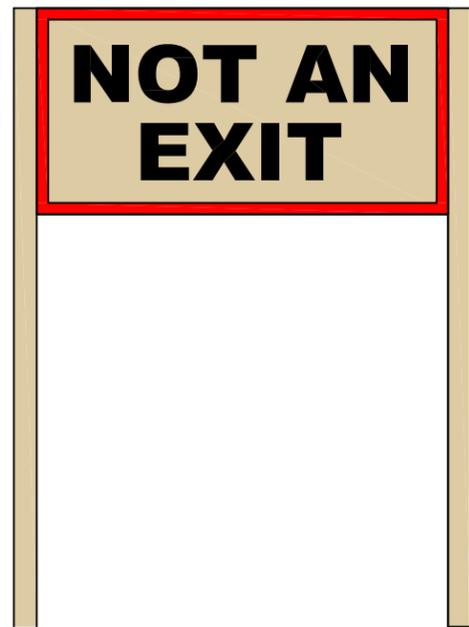
260TH ST E & FRANCE AVE  
ELKO, MN

#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

DRAWN BY	KMK
SCALE	MULTIPLE
PROJ. NO.	0001
DATE	2020-03-25
SHEET	CA4



SINGLE SIDED DIRECTIONAL SIGN  
WEST SIDE

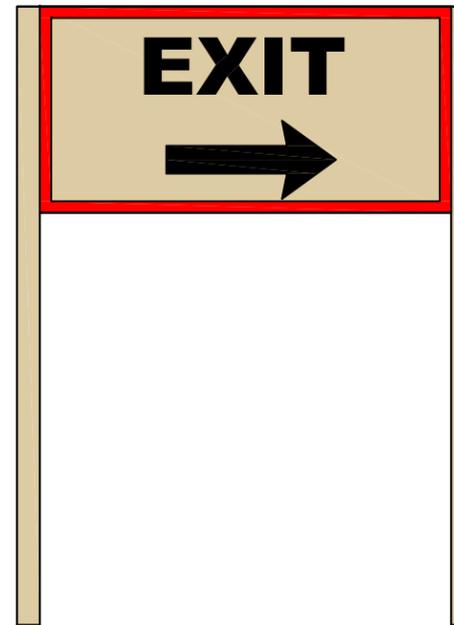


NON-LIT DIRECTIONAL SIGN  
RED & BLACK VINYL ON LIGHT BEIGE ALUMINUM  
1'-6"H X 3'-0"W X 4'-6"T= 4.50 SQ FT

**DIRECTIONAL SIGN #14**

SCALE: 3/4" = 1'-0"

SINGLE SIDED DIRECTIONAL SIGN  
WEST SIDE



NON-LIT DIRECTIONAL SIGN  
RED & BLACK VINYL ON LIGHT BEIGE ALUMINUM  
1'-6"H X 3'-0"W X 4'-6"T= 4.50 SQ FT

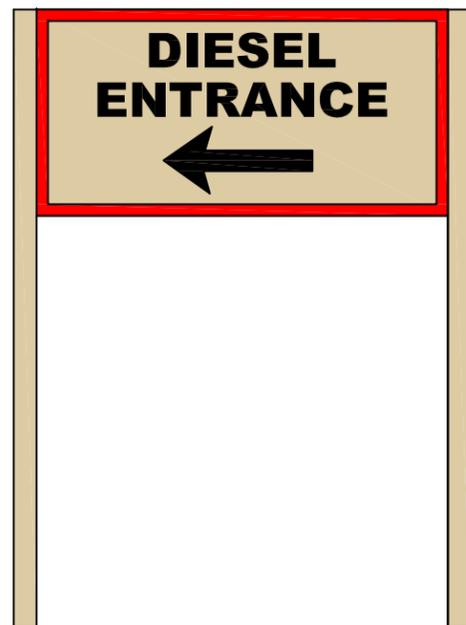
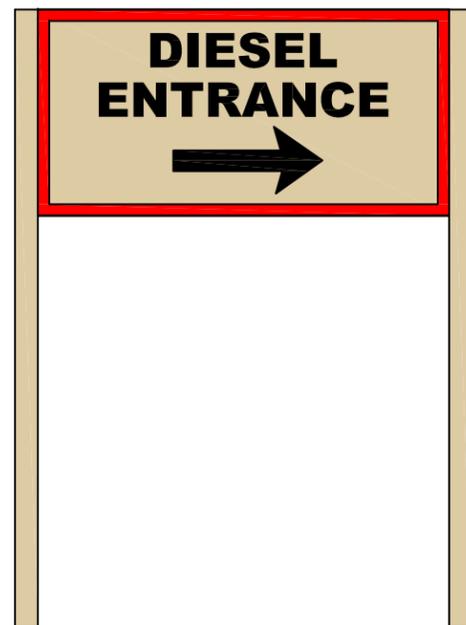
**DIRECTIONAL SIGN #15**

SCALE: 3/4" = 1'-0"

DOUBLE SIDED DIRECTIONAL SIGN

NORTH SIDE

SOUTH SIDE



NON-LIT DIRECTIONAL SIGN  
RED & BLACK VINYL ON LIGHT BEIGE ALUMINUM  
1'-6"H X 3'-0"W X 4'-6"T= 4.50 SQ FT

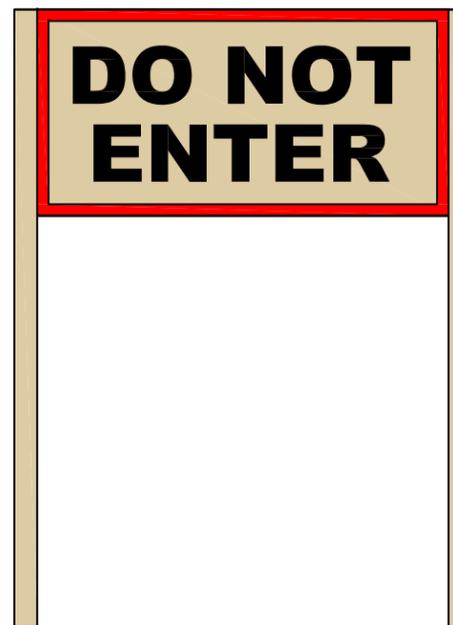
**DIRECTIONAL SIGN #16**

SCALE: 3/4" = 1'-0"

DOUBLE SIDED DIRECTIONAL SIGN

SOUTH SIDE

NORTH SIDE



NON-LIT DIRECTIONAL SIGN  
RED & BLACK VINYL ON LIGHT BEIGE ALUMINUM  
1'-6"H X 3'-0"W X 4'-6"T= 4.50 SQ FT

**DIRECTIONAL SIGN #17**

SCALE: 3/4" = 1'-0"



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DIRECTIONAL SIGNAGE  
CONVENIENCE STORE #1116  
w/ SIDE DIESEL & SINGLE BAY CW  
260TH ST E & FRANCE AVE  
ELKO, MN

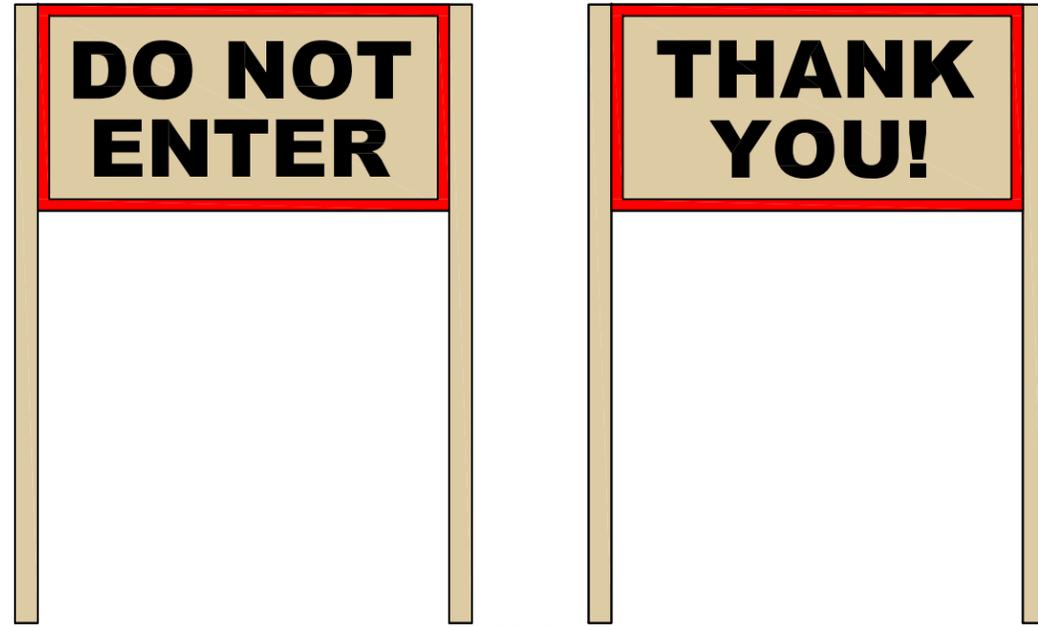
#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

DRAWN BY: KMK  
SCALE: MULTIPLE  
PROJ. NO.: 0001  
DATE: 2020-03-25  
SHEET: DS2

DOUBLE SIDED DIRECTIONAL SIGN

SOUTH SIDE

NORTH SIDE



NON-LIT DIRECTIONAL SIGN  
 RED & BLACK VINYL ON LIGHT BEIGE ALUMINUM  
 1'-6"H X 3'-0"W X 4'-6"T= 4.50 SQ FT

DIRECTIONAL SIGN #18

SCALE: 3/4" = 1'-0"

#19 KWIK TRIP FREESTANDING PYLON SIGN  
 SEE ATTACHMENT FROM LA CROSSE SIGN CO

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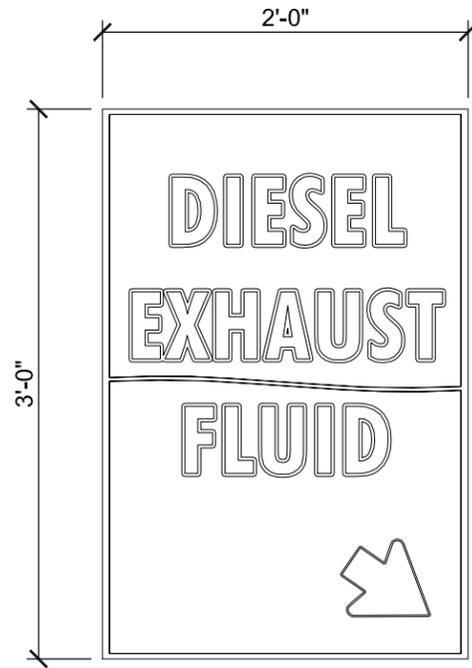
DIRECTIONAL SIGNAGE

CONVENIENCE STORE #1116  
 w/ SIDE DIESEL & SINGLE BAY CW

260TH ST E & FRANCE AVE  
 ELKO, MN

#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

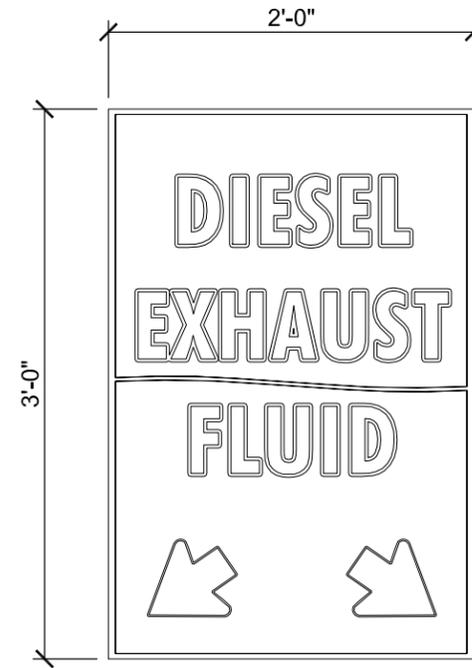
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 PROJ. NO.: 0001  
 DATE: 2020-03-25  
 SHEET: DS3



QTY: X

**DEF SIGN A**

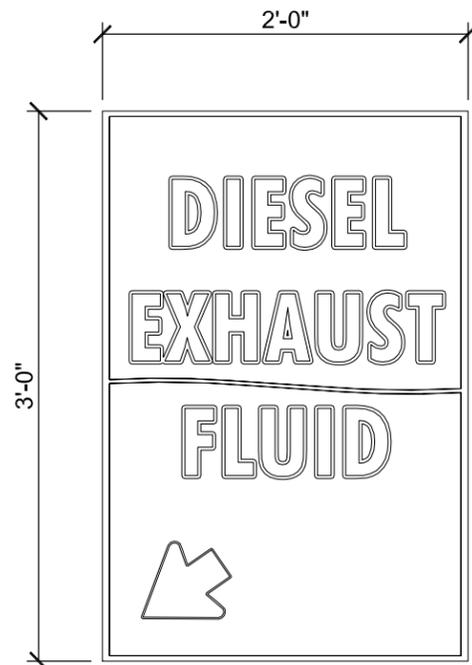
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QTY: X

**DEF SIGN B**

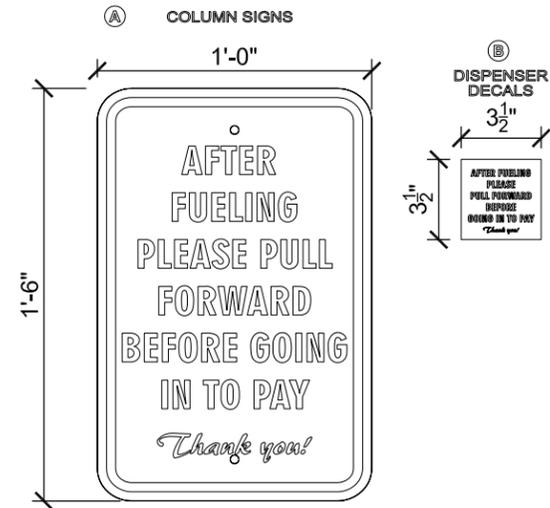
SCALE: 3/4" = 1'-0"



QTY: X

**DEF SIGN C**

SCALE: 3/4" = 1'-0"



**PULL AHEAD SIGN D**

SCALE: 1 1/2" = 1'-0"



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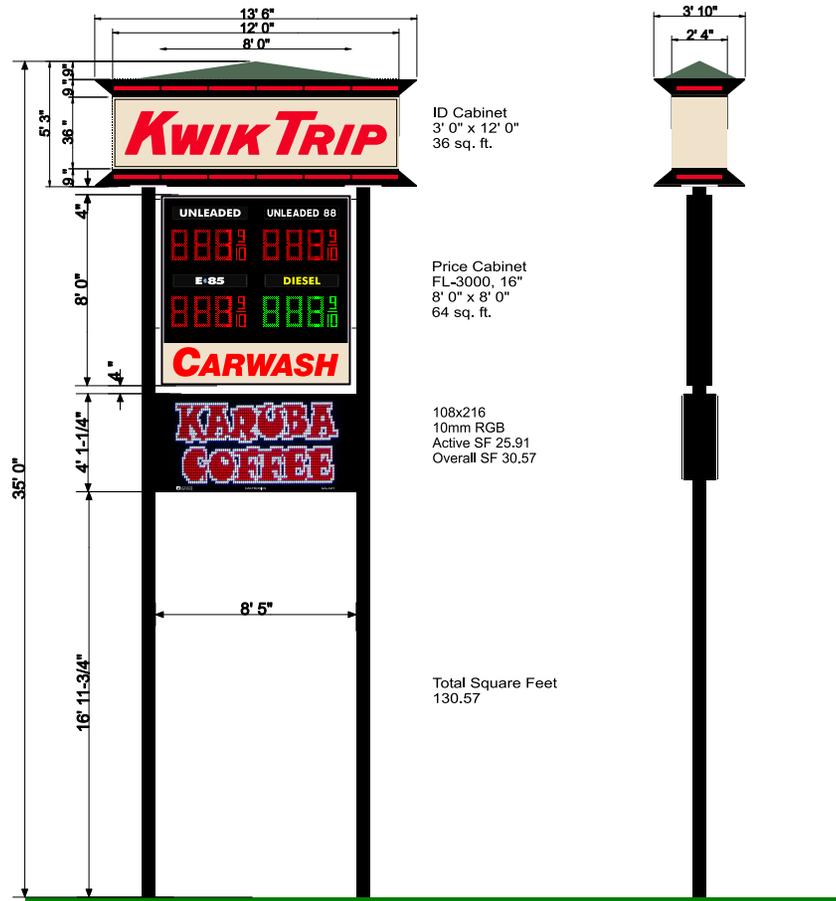
DEF / DSL SIGNAGE  
CONVENIENCE STORE #1116  
w/ SIDE DIESEL & SINGLE BAY CW  
260TH ST E & FRANCE AVE  
ELKO, MN

#	DATE	DESCRIPTION
1	2020/05/18	BKGD UPDATE

DRAWN BY: KMK  
SCALE: MULTIPLE  
PROJ. NO.: 0001  
DATE: 2020-03-25  
SHEET: DEF1

**DOUBLE FACED PYLON**

**1116 Elko, MN**



ID Cabinet  
3' 0" x 12' 0"  
36 sq. ft.

Price Cabinet  
FL-3000, 16"  
8' 0" x 8' 0"  
64 sq. ft.

108x216  
10mm RGB  
Active SF 25.91  
Overall SF 30.57

Total Square Feet  
130.57

Approved by: \_\_\_\_\_ Date: \_\_\_\_\_ Landlord: \_\_\_\_\_ Date: \_\_\_\_\_

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lacrossesign.com

DESIGN

SALES

FILE

COLOR KEY

\*COLORS ON SKETCH ARE ONLY A REPRESENTATION. ACTUAL COLOR OF FINISHED PRODUCT MAY DIFFER.



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2242 Mustang Way • Madison, WI 53718 • 608-222-5353  
2502 Melby Street • Eau Claire, WI 54703 • 715-835-6189

Drawing by: Danielle Hadley  
Sign Type: Pylon  
Date Created: 3/8/2019  
Last Modified:  
Scale: 1/8" = 1'

Job Name: Kwik Trip  
Job Address:  
Salesperson: Cindy Bluske  
Job Number:

Revision Number:  
Job File Location:

- 1 NA 208080 Beige
- 2 Black S/G paint
- 3 #2283 Red Acrylic
- 4 White of Acrylic
- 5 Red LED
- 6 NA 307880 Hemlock Green
- 7 Green LED
- 8 Black (230-22)
- 9 Yellow (230-015)
- 10 Blue (230-167)



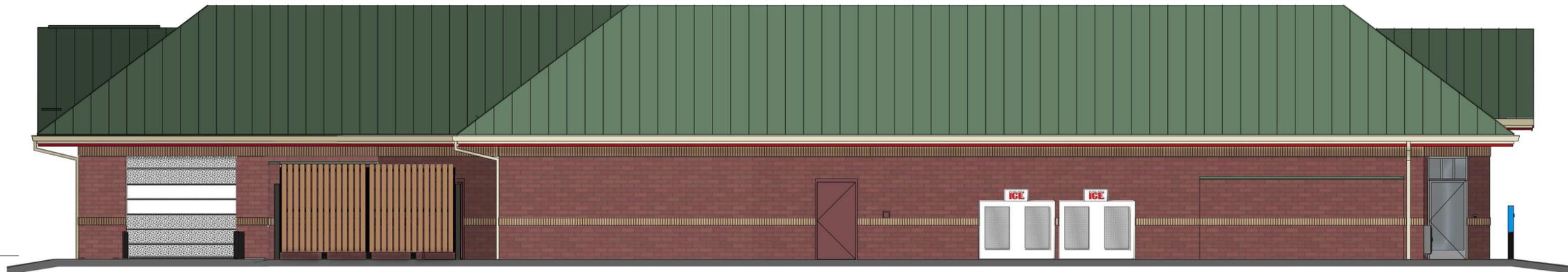
1 FRONT ELEVATION



2 RIGHT ELEVATION



3 REAR ELEVATION



4 LEFT ELEVATION

**Kwik  
TRIP**

**Kwik  
STAR**

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10 MPD

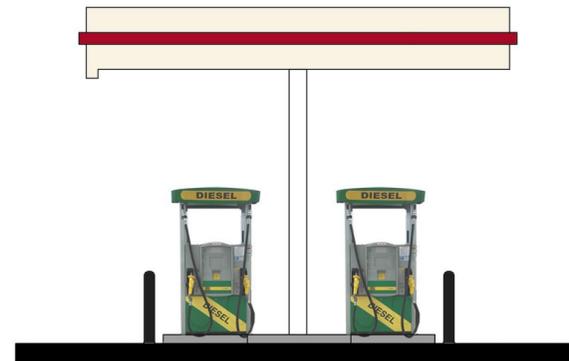
1 FRONT ELEVATION



2 SIDE ELEVATION



1 FRONT ELEVATION



2 SIDE ELEVATION



3 REAR ELEVATION

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**KWIK  
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2 LANE DIESEL

## DRAINAGE REPORT

---

May 15, 2020

**Kwik Trip Store 1116  
Elko Market Place  
Elko New Market, Minnesota**

### **Re: Kwik Trip Store 1116 – Stormwater Management Plan**

The purpose of this letter report is to discuss the proposed stormwater management system for the development of the convenience store lot in the Elko Market Place master plan of development. The site consists of Outlot A of the Elko Market Place 1<sup>st</sup> Addition plat. The site was previously graded as part of the overall mass grading for the Elko Market Place master plan of development. See Figure 1 for site location map.

#### **Existing Conditions**

The proposed Kwik Trip site is located in the southwest quadrant of the intersection of County Road 2 and France Avenue in Elko New Market, Scott County, Minnesota. The site is bounded on the north by County Road 2, on the east by France Avenue, on the south by undeveloped commercial property, and on the west by undeveloped commercial property and a United States Postal Service Building.

The proposed Kwik Trip site consists of Outlot A of the Elko Market Place 1<sup>st</sup> Addition plat and contains 4.08 acres. There is approximately 0.08 acres of existing impervious surface, which consists of a portion of the postal service parking lot and a small area in the northeast corner of the site.

#### **Proposed Conditions**

Kwik Trip, Inc. plans on developing the site into a convenience store with single bay carwash, two fueling canopies, and associated parking and drive areas. During construction of the Kwik Trip site, approximately 3.5 acres will be disturbed. After construction, the Kwik Trip lot will contain approximately 2.34 acres of impervious surface.

Storm sewer will be used to collect stormwater from the Kwik Trip site. The storm sewer will be connected to trunk storm sewer that was stubbed to the site as part of the overall Elko Market Place development. All stormwater from the developed portions of the site, except for some pervious areas along France Avenue and north of the proposed station store, will drain to trunk storm sewer that discharges to a regional pond south of the site for water quality treatment and rate control.

The Vermillion River is located approximately 0.6 miles north of the site and is an impaired water.

**Kwik Trip Stormwater Management Plan**

According to conversations with the City of Elko New Market, the existing regional pond located south of the site was designed to provide water quality treatment and rate control for the entire Elko Market Place in a fully developed condition. It is typical to design regional ponds for commercial development based on an assumed imperviousness of 75 percent. The proposed layout for the Kwik Trip site contains 2.34 acres of impervious surface, which is approximately 57 percent of the area of the site. Because the proposed imperviousness of the Kwik Trip site is well below typical commercial development, it is our opinion that the regional pond will provide adequate water quality treatment and rate control for the site. The Elko New Market stormwater management plan was not available for review at the time of this report. The Elko New Market stormwater management plan should be reviewed to determine actual design parameters and to verify the regional pond capacity.

The MPCA Construction Stormwater Permit requires sites to consider volume reduction practices where practical. The MPCA Construction Stormwater Permit prohibits the construction of infiltration systems that receive runoff from vehicle fueling areas. The MPCA Construction Stormwater Permit also prohibits infiltration of stormwater in areas of predominantly Hydrologic Soil Group “D”. The proposed site will have vehicle fueling onsite and onsite soils consist primarily of clay, therefore infiltration is prohibited on site

**Storm Sewer Design**

Storm sewer will be used to collect stormwater from the site. The storm sewer will connect to a 36-inch existing pipe in the southeast corner of the site. The rational method was used to determine anticipated flows to each catch basin and the pipes were sized using those flows along with Manning’s equation. All onsite storm sewers were designed to accommodate a 10-year storm at a minimum. See attached Figure 2 for Storm Sewer Drainage Map and attached rational method spreadsheet for design calculations.

**Summary**

The proposed Kwik Trip lot will meet the overall stormwater treatment requirements from RCWD and the MPCA Construction Stormwater Permit. Adequate water quality treatment and rate control is provided by the existing regional pond located south of the site. All storm sewers have been sized to accommodate a 10-year storm at a minimum. The proposed development will not have an adverse impact on the existing stormwater infrastructure originally planned for the development of the site.

**Certification**

I hereby certify that this plan, specification or report was prepared by me or under my direction supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.



\_\_\_\_\_  
Joseph T. Radach, P.E.  
License #: 45889

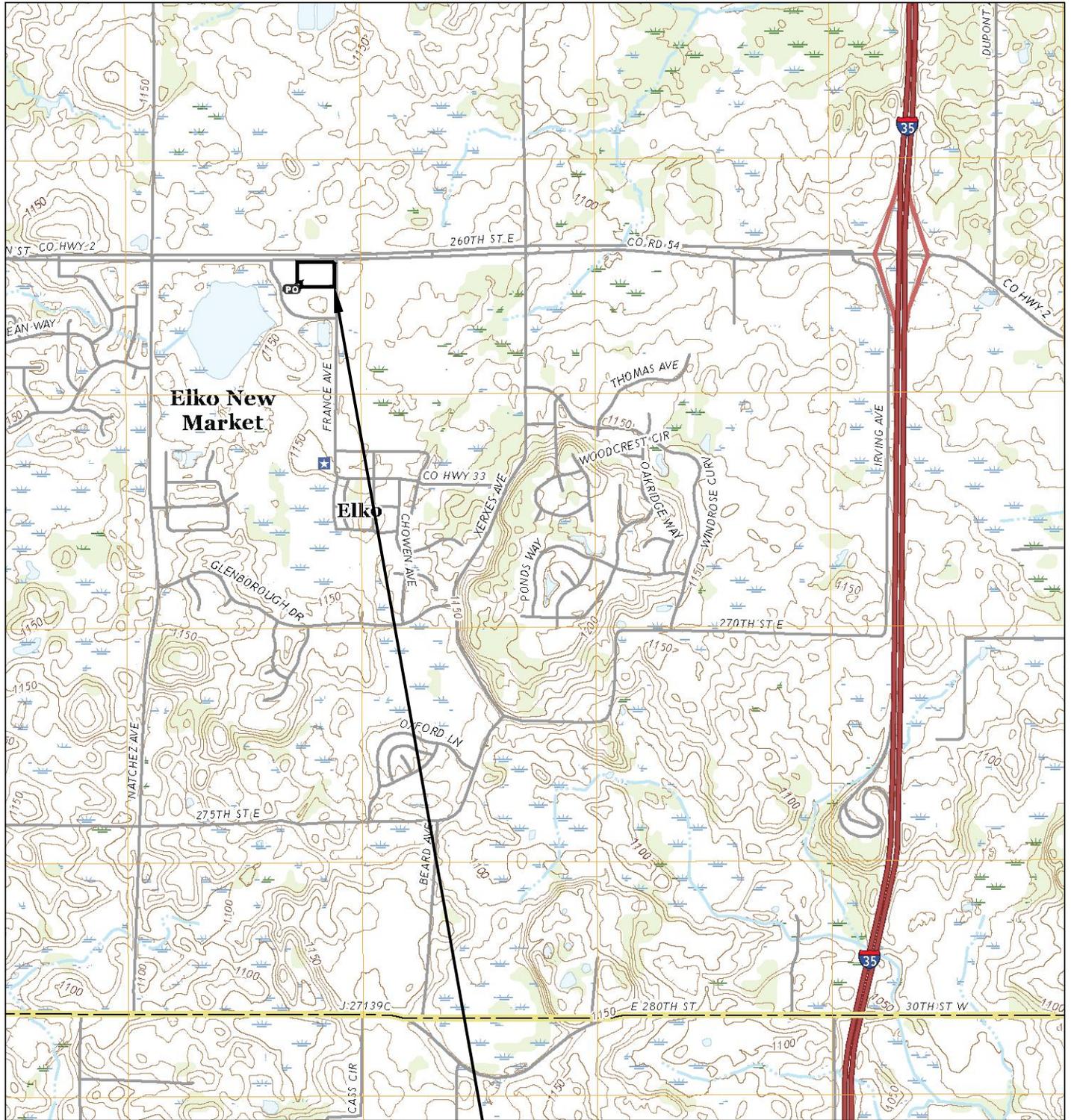
05/15/20

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Date

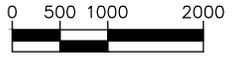
**Attachments**

- Figure 1: Site Location Map
- Figure 2: Storm Sewer Drainage Map
- Attachment 1: Storm Sewer Design Spreadsheet
- Attachment 2: Soil Borings

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SITE LOCATION



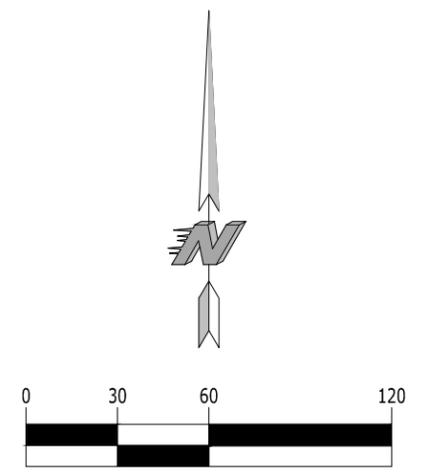
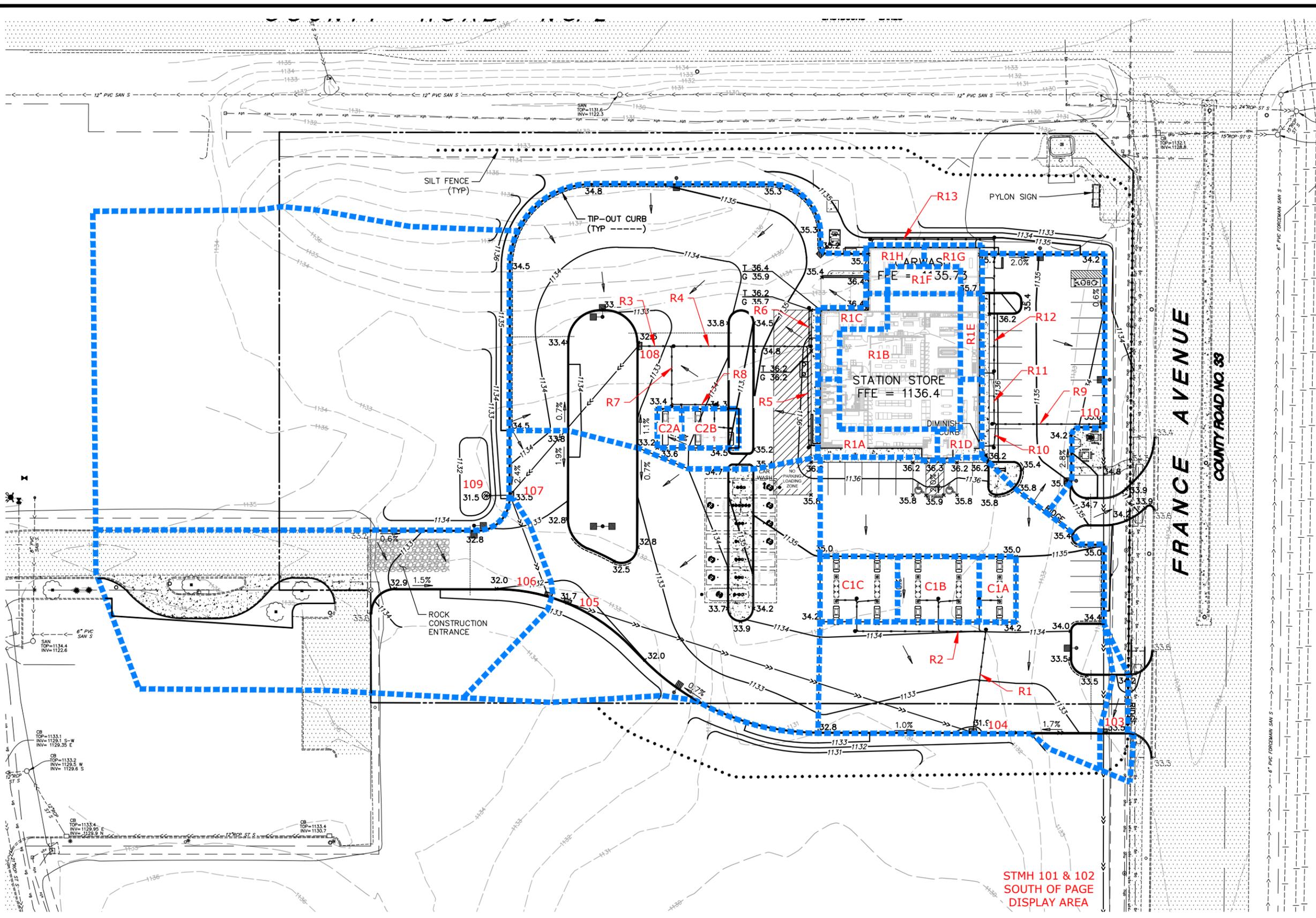
SOURCE: USGS NEW MARKET 7.5 MIN. QUADRANGLE



**KWIK TRIP STORE 1116**  
**W/ 1-BAY CARWASH**  
Elko New Market,  
Minnesota

**FIGURE 1**  
**SITE LOCATION MAP**

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### LEGEND

- - - - - CATCHMENT LINE
- 101 CATCHMENT

STMH 101 & 102  
SOUTH OF PAGE  
DISPLAY AREA



Notes:

**KWIK TRIP, INC.**  
1626 Oak Street  
La Crosse, WI 54602

**KWIK TRIP STORE 1116**  
**W/ 1-BAY CARWASH**  
Elko New Market, Minnesota

**FIGURE 2**  
**STORM SEWER**  
**DRAINAGE MAP**

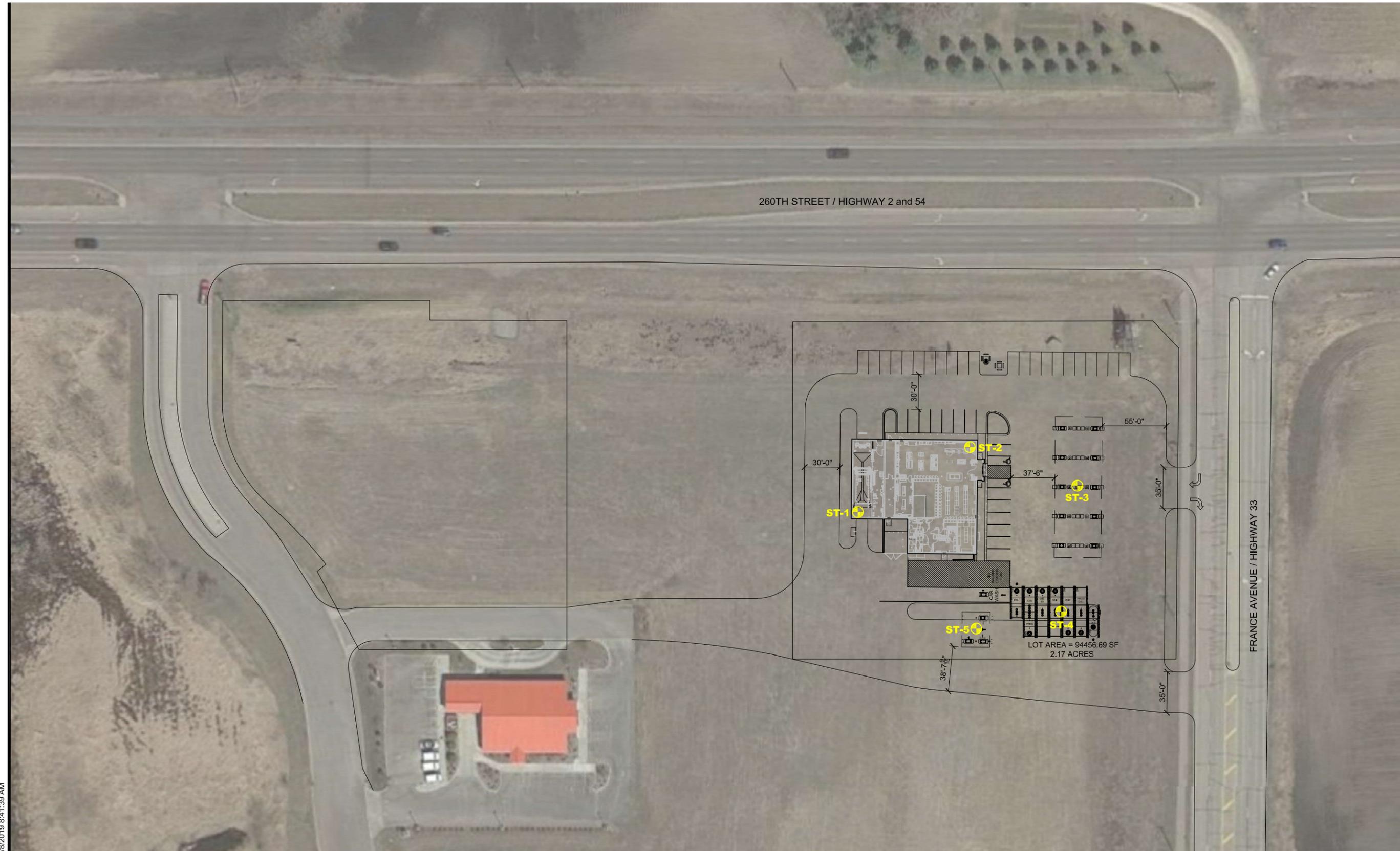
PROJECT NAME: Kwik Trip Convenience Store 1116  
 PROJECT LOCATION: Elko New Market, MN  
 CARLSON PROJECT NO.: 8622  
 DATE: 05/15/20  
 BY: JTR

DESIGN CRITERIA	
Storm Frequency	10 year
Manning's "n"	0.013
"C" coefficient	varies



**PROPOSED CONDITIONS STORM SEWER DESIGN**

LOCATION		MH SIZE	AREA			"C" COEFFICIENT			GENERAL		DESIGN						PROFILE INFORMATION			
CBMH (from)	CBMH (to)	Diameter (in)	Impervious (sf)	Pervious (sf)	Total Area (ac)	Inc. "C"	Inc. CA	Cum. CA	T (min)	I (in/hr)	Q = CAI (cfs)	D (in)	% Grade	Q <sub>full</sub> (cfs)	V <sub>full</sub> (fps)	L (ft)	Invert (in)	Invert (out)	Rim El.	Build (ft)
C1C	R2	na	2000	0	0.05	0.90	0.04	0.04	10	5.8	0.24	4	2.00%	0.27	3.08	33	1129.64	1128.98	1134.50	4.86
C1B	R2	na	1900	0	0.04	0.90	0.04	0.04	10	5.8	0.23	4	2.00%	0.27	3.08	33	1129.11	1128.45	1134.50	5.39
C1A	R2	na	900	0	0.02	0.90	0.02	0.02	10	5.8	0.11	4	2.00%	0.27	3.08	33	1128.86	1128.20	1134.50	5.64
C2B	R8	na	840	0	0.02	0.90	0.02	0.02	10	5.8	0.10	4	2.00%	0.27	3.08	26	1130.51	1129.99	1134.23	3.72
C2A	R8	na	360	0	0.01	0.90	0.01	0.01	10	5.8	0.04	4	2.00%	0.27	3.08	26	1130.29	1129.77	1133.82	3.53
R1H	R13	na	687	0	0.02	0.90	0.01	0.01	10	5.8	0.08	6	2.00%	0.79	4.04	5	1129.02	1128.92	1135.73	6.71
R1G	R13	na	687	0	0.02	0.90	0.01	0.01	10	5.8	0.08	6	2.00%	0.79	4.04	5	1130.01	1129.91	1135.73	5.72
R1F	R12	na	738	0	0.02	0.90	0.02	0.02	10	5.8	0.09	6	2.00%	0.79	4.04	9	1130.04	1129.86	1135.73	5.69
R1E	R11	na	691	0	0.02	0.90	0.01	0.01	10	5.8	0.08	6	2.00%	0.79	4.04	9	1129.81	1129.63	1136.40	6.59
R1D	R10	na	893	0	0.02	0.90	0.02	0.02	10	5.8	0.11	6	2.00%	0.79	4.04	9	1129.68	1129.50	1136.40	6.72
R1C	R6	na	1078	0	0.02	0.90	0.02	0.02	10	5.8	0.13	6	2.00%	0.79	4.04	10	1130.90	1130.70	1136.40	5.50
R1B	R5	na	5494	0	0.13	0.90	0.11	0.11	10	5.8	0.66	6	2.00%	0.79	4.04	7	1130.55	1130.41	1136.40	5.85
R1A	R5	na	1684	0	0.04	0.90	0.03	0.03	10	5.8	0.20	6	2.00%	0.79	4.04	7	1130.24	1130.10	1136.40	6.16
R13	R11	na	0	0	0.00	0.00	0.00	0.03	10	5.8	0.16	10	1.00%	2.19	4.01	76	1130.66	1129.90	1135.00	4.34
R12	R10	na	0	0	0.00	0.00	0.00	0.04	10	5.8	0.25	10	1.00%	2.19	4.01	82	1129.90	1129.08	1135.00	5.10
R11	R8	na	0	0	0.00	0.00	0.00	0.06	10	5.8	0.34	10	1.00%	2.19	4.01	33	1129.08	1128.75	1136.10	7.02
R10	R8	na	0	0	0.00	0.00	0.00	0.02	10	5.8	0.11	10	1.00%	2.19	4.01	15	1128.87	1128.72	1136.10	7.23
R9	R10	na	0	0	0.00	0.00	0.00	0.08	10	5.8	0.44	10	1.00%	2.19	4.01	68	1128.75	1128.07	1136.10	7.35
R8	R7	na	0	0	0.00	0.00	0.00	0.02	10	5.8	0.14	10	1.00%	2.19	4.01	28	1129.74	1129.46	1134.10	4.36
R7	R3	na	0	0	0.00	0.00	0.00	0.02	10	5.8	0.14	10	1.00%	2.19	4.01	37	1129.46	1129.09	1133.50	4.04
R6	R4	na	0	0	0.00	0.00	0.00	0.02	10	5.8	0.13	10	1.00%	2.19	4.01	24	1130.17	1129.93	1135.70	5.53
R5	R4	na	0	0	0.00	0.00	0.00	0.15	10	5.8	0.86	10	1.00%	2.19	4.01	63	1130.53	1129.90	1136.20	5.67
R4	R3	na	0	0	0.00	0.00	0.00	0.17	10	5.8	0.99	10	1.00%	2.19	4.01	84	1129.93	1129.09	1136.20	6.27
R3	R108	na	0	0	0.00	0.00	0.00	0.20	10	5.8	1.13	10	1.00%	2.19	4.01	22	1129.09	1128.87	1133.10	4.01
R2	R1	na	0	0	0.00	0.00	0.00	0.10	10	5.8	0.58	10	1.00%	2.19	4.01	81	1128.73	1127.92	1134.10	5.37
R1	R104	na	0	0	0.00	0.00	0.00	0.10	10	5.8	0.58	10	1.00%	2.19	4.01	63	1127.92	1127.29	1134.00	6.08
110	103	48	9840	224	0.23	0.88	0.20	0.28	10	5.8	1.63	15	0.50%	4.56	3.72	189	1127.79	1126.85	1133.43	5.64
109	107	27	24351	24351	1.12	0.55	0.61	0.61	14	5.1	3.14	15	0.50%	4.56	3.72	15	1128.16	1128.09	1131.50	3.41
108	107	24x36	25846	4232	0.69	0.80	0.55	0.75	10	5.8	4.34	15	0.50%	4.56	3.72	120	1128.69	1128.09	1132.33	3.64
107	106	48	0	0	0.00	0.00	0.00	1.36	14	5.1	6.96	18	0.45%	7.04	3.98	66	1127.92	1127.63	1133.33	5.41
106	105	48	18203	7064	0.58	0.70	0.41	1.77	14	5.1	9.04	24	0.25%	11.30	3.60	12	1127.32	1127.29	1131.65	4.33
105	104	48	21322	7427	0.66	0.72	0.47	2.25	14	5.1	11.46	24	0.30%	12.38	3.94	261	1127.29	1126.51	1131.53	4.24
104	103	60	21687	1211	0.53	0.86	0.45	2.80	14	5.1	14.28	30	0.20%	18.33	3.73	80	1126.18	1126.02	1131.73	5.55
103	102	72	4359	890	0.12	0.78	0.09	3.17	14	5.1	16.19	30	0.20%	18.33	3.73	307	1126.02	1125.40	1132.15	6.13
102	101	72	0	0	0.00	0.00	0.00	3.17	14	5.1	16.19	30	0.20%	18.33	3.73	106	1125.40	1125.19	1133.20	7.80
101	ex	72	0	0	0.00	0.00	0.00	3.17	14	5.1	16.19	30	0.20%	18.33	3.73	0	1125.19	1125.19	1130.80	5.61



Drawing Information

Project No:  
B1901623

Drawing No:  
B1910623

Drawn By: JAG  
Date Drawn: 10/14/19  
Checked By: NMR  
Last Modified: 11/8/19

Project Information

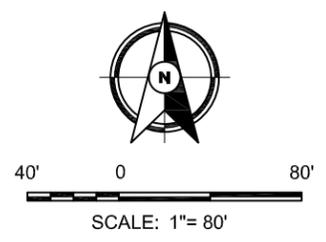
Proposed Kwik Trip  
#1116

260th Street and France  
Avenue

Elko New Market,  
Minnesota

Soil Boring  
Location Sketch

DENOTES APPROXIMATE LOCATION OF STANDARD PENETRATION TEST BORING



<b>Project Number B1910623</b>					<b>BORING: ST-1</b>		
<b>Geotechnical &amp; Environmental Evaluation</b>					LOCATION: See attached sketch. Hit obstruction at 7 1/2 to 10 feet. Offset 5 feet West.		
<b>Kwik Trip #1116 - Elko New Market, MN</b>					NORTHING: 136620		EASTING: 496516
<b>SW corner of 230th Street East &amp; France Avenue</b>					START DATE: 10/21/19		END DATE: 10/21/19
<b>Elko New Market, Minnesota</b>					SURFACING: Grass		WEATHER: Rain
DRILLER: M. Barber		LOGGED BY: B. Wright			SURFACE ELEVATION: 1132.1 ft		RIG: _____
		METHOD: 3 1/4" HSA					
Elev./Depth ft	Water Level	Description of Materials (Soil-ASTM D2488 or 2487; Rock-USACE EM 1110-1-2908)	Sample	Blows (N-Value) Recovery	PID ppm	MC %	Tests or Remarks
1131.1 1.0		LEAN CLAY (CL), with roots, black, moist (TOPSOIL)					Boring elevations and surface elevations were measured with GPS technology.
		LEAN CLAY (CL), trace Sand, gray, wet, soft to very soft (ALLUVIUM)	5	0-2-2 (4) WOH/6" 6"	0.1	38	
1125.1 7.0		CLAYEY SAND (SC), trace Gravel, mottled gray, wet, medium to very stiff (GLACIAL TILL)	10	0-0-0 WOH/18" 12"	0.1		Soil sample collected from 5 to 7 1/2 feet at 11:10.
			15	0-4-2 (6) WOH/6" 18"	0.2		P200=47%
			20	1-2-3 (5) 18"	0.3	18	
			25	2-3-4 (7) 18"	0.3		
1111.1 21.0		END OF BORING	30	4-7-8 (15) 16"	0.2		Water sample collected at 11 1/2 feet at 12:15.
		Boring immediately backfilled with bentonite grout	35	10-9-10 (19) 15"	0.1		
			40				Water observed at 11.0 feet while drilling.

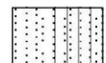
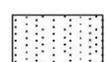
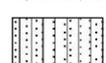
<b>Project Number B1910623</b>					<b>BORING: ST-2</b>		
<b>Geotechnical &amp; Environmental Evaluation</b>					LOCATION: See attached sketch		
<b>Kwik Trip #1116 - Elko New Market, MN</b>					NORTHING: 136675 EASTING: 496612		
<b>SW corner of 230th Street East &amp; France Avenue</b>					START DATE: 10/21/19 END DATE: 10/21/19		
<b>Elko New Market, Minnesota</b>					SURFACING: Grass WEATHER: Rain		
DRILLER: M. Barber		LOGGED BY: B. Wright		SURFACE ELEVATION: 1132.4 ft		RIG: METHOD: 3 1/4" HSA	
Elev./Depth ft	Water Level	Description of Materials (Soil-ASTM D2488 or 2487; Rock-USACE EM 1110-1-2908)	Sample	Blows (N-Value) Recovery	PID ppm	MC %	Tests or Remarks
1131.4		LEAN CLAY (CL), with roots, black, moist (TOPSOIL)					
1.0		LEAN CLAY with SAND (CL), trace Gravel, gray, wet, soft (ALLUVIUM)		0-2-2 (4) WOH/6" 10"	0.2		qp = 1 tsf Water sample collected at 2 1/2 feet at 13:20.
1128.4		CLAYEY SAND (SC), trace Gravel, mottled gray, wet, medium (GLACIAL TILL)	5	1-2-3 (5) 18"	0.2		Soil sample collected from 5 to 7 1/2 feet at 12:41.
				2-3-5 (8) 18"	0.2	16	P200=43%
1122.4		POORLY GRADED SAND (SP), medium to coarse-grained Sand, with Gravel, brown, wet, medium dense (GLACIAL OUTWASH)	10	3-6-7 (13) 18"	0.2		
				3-5-6 (11) 16"	0.2		
1118.4		SILTY SAND (SM), fine-grained Sand, grayish brown, wet, medium dense (GLACIAL OUTWASH)	15	4-6-7 (13) 8"	0.1		
1114.4		CLAYEY SAND (SC), trace Gravel, reddish brown, moist, very stiff (GLACIAL TILL)	20	7-10-18 (28) 17"	0.1		1 inch PVC temporary well installed.
1111.4		END OF BORING					Water observed at 2.0 feet while drilling.
21.0		Boring immediately backfilled with bentonite grout					
			25				
			30				

<b>Project Number B1910623</b>					<b>BORING: ST-3</b>		
<b>Geotechnical &amp; Environmental Evaluation</b>					LOCATION: See attached sketch		
<b>Kwik Trip #1116 - Elko New Market, MN</b>					NORTHING: 136642	EASTING: 496704	
<b>SW corner of 230th Street East &amp; France Avenue</b>					START DATE: 10/21/19	END DATE: 10/21/19	
<b>Elko New Market, Minnesota</b>					SURFACING: Grass WEATHER: Rain		
DRILLER: M. Barber		LOGGED BY: B. Wright					
SURFACE ELEVATION: 1132.3 ft		RIG:		METHOD: 3 1/4" HSA			
Elev./ Depth ft	Water Level	Description of Materials (Soil-ASTM D2488 or 2487; Rock-USACE EM 1110-1-2908)	Sample	Blows (N-Value) Recovery	PID ppm	MC %	Tests or Remarks
1131.3		LEAN CLAY (CL), with roots, black, moist (TOPSOIL)					
1.0		LEAN CLAY with SAND (CL), gray, moist, soft (ALLUVIUM)		2-2-2 (4) 15"	0.0	29	P200=74% Water sample collected at 3 feet at 16:25.
1128.3		CLAYEY SAND (SC), trace Gravel, mottled grayish brown, moist, medium to very stiff (GLACIAL TILL)	5	2-2-3 (5) 13"	0.1		Soil sample collected from 5 to 7 1/2 feet at 15:56.
4.0				2-2-3 (5) 13"	0.1		
				10	11-14-9 (23) 7"		
1121.3		CLAYEY SAND (SC), trace Gravel, dark gray, moist, very stiff (GLACIAL TILL)					*No recovery **Not screened
11.0			15				*No recovery **Not screened
				20	6-10-14 (24) 16"	0.0	
1111.3		END OF BORING					Water observed at 6.0 feet while drilling.
21.0		Boring immediately backfilled with bentonite grout					

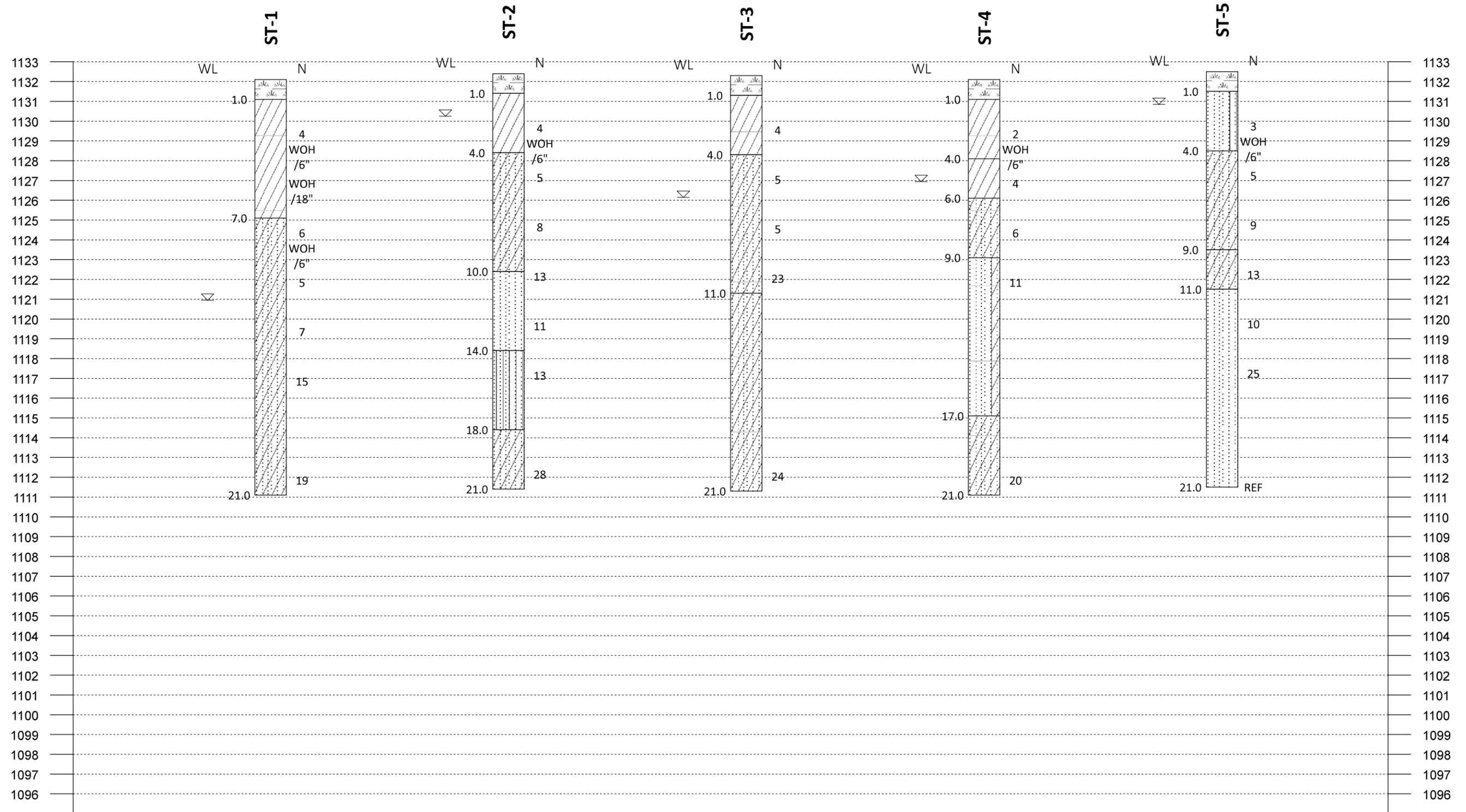
<b>Project Number B1910623</b>					<b>BORING: ST-4</b>		
<b>Geotechnical &amp; Environmental Evaluation</b>					LOCATION: See attached sketch		
<b>Kwik Trip #1116 - Elko New Market, MN</b>					NORTHING: 136535 EASTING: 496690		
<b>SW corner of 230th Street East &amp; France Avenue</b>					START DATE: 10/21/19 END DATE: 10/21/19		
<b>Elko New Market, Minnesota</b>					SURFACING: Grass WEATHER: Rain		
DRILLER: M. Barber		LOGGED BY: B. Wright		SURFACE ELEVATION: 1132.1 ft		RIG: METHOD: 3 1/4" HSA	
Elev./Depth ft	Water Level	Description of Materials (Soil-ASTM D2488 or 2487; Rock-USACE EM 1110-1-2908)	Sample	Blows (N-Value) Recovery	PID ppm	MC %	Tests or Remarks
1131.1		LEAN CLAY (CL), with roots, black, moist (TOPSOIL)					
1.0		LEAN CLAY (CL), gray, moist, soft (ALLUVIUM)		0-1-1 (2) WOH/6" 17"	0.1		Water sample collected at 1 1/2 feet at 14:15. qp = 1 tsf
1128.1		LEAN CLAY (CL), mottled gray, wet, soft (ALLUVIUM)	5	1-1-3 (4) 15"	0.4		Soil sample collected from 5 to 7 1/2 feet at 13:46.
1126.1		CLAYEY SAND (SC), mottled gray, wet, medium (GLACIAL TILL)		3-3-3 (6) 17"	0.3		
1123.1		POORLY GRADED SAND with CLAY (SP-SC), medium-grained Sand, reddish brown, wet, medium dense (GLACIAL OUTWASH)	10	7-5-6 (11) 18"	0.2		
			15			20	*No recovery **Not screened P200=8%
1115.1		CLAYEY SAND (SC), trace Gravel, dark gray, moist, very stiff (GLACIAL TILL)					*No recovery **Not screened
17.0			20	9-9-11 (20) 18"	0.0		qp = >4 tsf
1111.1		END OF BORING					Water observed at 5.0 feet while drilling.
21.0		Boring immediately backfilled with bentonite grout					
			25				
			30				

<b>Project Number B1910623</b>					<b>BORING: ST-5</b>		
<b>Geotechnical &amp; Environmental Evaluation</b>					LOCATION: See attached sketch		
<b>Kwik Trip #1116 - Elko New Market, MN</b>					NORTHING: 136520      EASTING: 496617		
<b>SW corner of 230th Street East &amp; France Avenue</b>					START DATE: 10/21/19      END DATE: 10/21/19		
<b>Elko New Market, Minnesota</b>					SURFACING: Grass      WEATHER: Rain		
DRILLER: M. Barber		LOGGED BY: B. Wright					
SURFACE ELEVATION: 1132.5 ft		RIG:		METHOD: 3 1/4" HSA			
Elev./Depth ft	Water Level	Description of Materials (Soil-ASTM D2488 or 2487; Rock-USACE EM 1110-1-2908)	Sample	Blows (N-Value) Recovery	PID ppm	MC %	Tests or Remarks
1131.5	☒	LEAN CLAY (CL), with roots, black, moist (TOPSOIL)					
1.0		POORLY GRADED SAND with SILT (SP-SM), fine to medium-grained Sand, reddish brown, moist, very loose (ALLUVIUM)	☒	0-1-2 (3) WOH/6" 18"	0.0		Water sample collected at 2 1/2 feet at 15:30.
1128.5		CLAYEY SAND (SC), trace Gravel, mottled grayish brown, moist, medium to stiff (GLACIAL TILL)	5 ☒	2-2-3 (5) 17"	0.0	14	Soil sample collected from 5 to 7 1/2 feet at 14:58. P200=37%
1123.5			☒	2-3-6 (9) 18"	0.1		
1121.5		CLAYEY SAND (SC), brown, moist, stiff (GLACIAL TILL)	10 ☒	3-5-8 (13) 18"	0.1		
11.0		POORLY GRADED SAND (SP), medium to coarse-grained Sand, trace Gravel, brown, moist, loose to very dense (GLACIAL OUTWASH)	☒	3-3-7 (10) 16"	0.1		
			15 ☒	12-11-14 (25) 15"	0.1		
1111.5			20				*No recovery **Not screened
21.0		END OF BORING		50/0" (REF)			Water observed at 1.5 feet while drilling.
		Boring immediately backfilled with bentonite grout					
			25				
			30				

**Legend Key**

-  Topsoil
-  SP-SM
-  CL
-  SC
-  SP-SC
-  SP
-  SM

1095.00

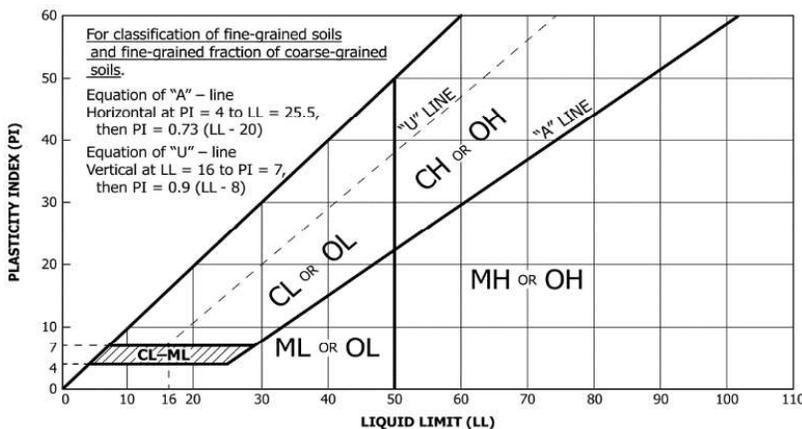


Project ID: B1910623  
 Vert. Scale: 1"= 5"  
 Hor. Scale: NTS  
 Date: 12-05-2019

Fence Diagram  
 Geotechnical & Environmental Evaluation  
 Kwik Trip #1116 - Elko New Market, MN  
 SW corner of 230th Street East & France Avenue  
 Elko New Market, Minnesota

Criteria for Assigning Group Symbols and Group Names Using Laboratory Tests <sup>A</sup>			Soil Classification		
			Group Symbol	Group Name <sup>B</sup>	
Coarse-grained Soils (more than 50% retained on No. 200 sieve)	Gravels (More than 50% of coarse fraction retained on No. 4 sieve)	Clean Gravels (Less than 5% fines <sup>C</sup> )	$C_u \geq 4$ and $1 \leq C_c \leq 3^D$	GW	Well-graded gravel <sup>E</sup>
		Gravels with Fines (More than 12% fines <sup>C</sup> )	$C_u < 4$ and/or ( $C_c < 1$ or $C_c > 3$ ) <sup>D</sup>	GP	Poorly graded gravel <sup>E</sup>
			Fines classify as ML or MH	GM	Silty gravel <sup>EFG</sup>
	Sands (50% or more coarse fraction passes No. 4 sieve)	Clean Sands (Less than 5% fines <sup>H</sup> )	$C_u \geq 6$ and $1 \leq C_c \leq 3^D$	SW	Well-graded sand <sup>I</sup>
		Sands with Fines (More than 12% fines <sup>H</sup> )	$C_u < 6$ and/or ( $C_c < 1$ or $C_c > 3$ ) <sup>D</sup>	SP	Poorly graded sand <sup>I</sup>
			Fines classify as ML or MH	SM	Silty sand <sup>FGI</sup>
	Fines classify as CL or CH	SC	Clayey sand <sup>FGI</sup>		
Fine-grained Soils (50% or more passes the No. 200 sieve)	Silt and Clays (Liquid limit less than 50)	Inorganic	PI > 7 and plots on or above "A" line <sup>J</sup>	CL	Lean clay <sup>KLM</sup>
			PI < 4 or plots below "A" line <sup>J</sup>	ML	Silt <sup>KLM</sup>
	Silt and Clays (Liquid limit 50 or more)	Inorganic	PI plots on or above "A" line	CH	Fat clay <sup>KLM</sup>
			PI plots below "A" line	MH	Elastic silt <sup>KLM</sup>
		Organic	Liquid Limit – oven dried	OL	Organic clay <sup>KLMN</sup>
			Liquid Limit – not dried < 0.75	OH	Organic silt <sup>KLMQ</sup>
Highly Organic Soils	Primarily organic matter, dark in color, and organic odor		PT	Peat	

- A. Based on the material passing the 3-inch (75-mm) sieve.
- B. If field sample contained cobbles or boulders, or both, add "with cobbles or boulders, or both" to group name.
- C. Gravels with 5 to 12% fines require dual symbols:  
GW-GM well-graded gravel with silt  
GW-GC well-graded gravel with clay  
GP-GM poorly graded gravel with silt  
GP-GC poorly graded gravel with clay
- D.  $C_u = D_{60} / D_{10}$        $C_c = (D_{30})^2 / (D_{10} \times D_{60})$
- E. If soil contains  $\geq 15\%$  sand, add "with sand" to group name.
- F. If fines classify as CL-ML, use dual symbol GC-GM or SC-SM.
- G. If fines are organic, add "with organic fines" to group name.
- H. Sands with 5 to 12% fines require dual symbols:  
SW-SM well-graded sand with silt  
SW-SC well-graded sand with clay  
SP-SM poorly graded sand with silt  
SP-SC poorly graded sand with clay
- I. If soil contains  $\geq 15\%$  gravel, add "with gravel" to group name.
- J. If Atterberg limits plot in hatched area, soil is CL-ML, silty clay.
- K. If soil contains 15 to < 30% plus No. 200, add "with sand" or "with gravel", whichever is predominant.
- L. If soil contains  $\geq 30\%$  plus No. 200, predominantly sand, add "sandy" to group name.
- M. If soil contains  $\geq 30\%$  plus No. 200 predominantly gravel, add "gravelly" to group name.
- N. PI  $\geq 4$  and plots on or above "A" line.
- O. PI < 4 or plots below "A" line.
- P. PI plots on or above "A" line.
- Q. PI plots below "A" line.



Laboratory Tests			
DD	Dry density, pcf	OC	Organic content, %
WD	Wet density, pcf	q <sub>p</sub>	Pocket penetrometer strength, tsf
P200	% Passing #200 sieve	MC	Moisture content, %
		q <sub>u</sub>	Unconfined compression test, tsf
		LL	Liquid limit
		PL	Plastic limit
		PI	Plasticity index

**Particle Size Identification**

- Boulders..... over 12"
- Cobbles..... 3" to 12"
- Gravel  
Coarse..... 3/4" to 3" (19.00 mm to 75.00 mm)  
Fine..... No. 4 to 3/4" (4.75 mm to 19.00 mm)
- Sand  
Coarse..... No. 10 to No. 4 (2.00 mm to 4.75 mm)  
Medium..... No. 40 to No. 10 (0.425 mm to 2.00 mm)  
Fine..... No. 200 to No. 40 (0.075 mm to 0.425 mm)
- Silt..... No. 200 (0.075 mm) to .005 mm
- Clay..... < .005 mm

**Relative Proportions<sup>L, M</sup>**

- trace..... 0 to 5%
- little..... 6 to 14%
- with.....  $\geq 15\%$

**Inclusion Thicknesses**

- lens..... 0 to 1/8"
- seam..... 1/8" to 1"
- layer..... over 1"

**Apparent Relative Density of Cohesionless Soils**

- Very loose ..... 0 to 4 BPF
- Loose ..... 5 to 10 BPF
- Medium dense..... 11 to 30 BPF
- Dense..... 31 to 50 BPF
- Very dense..... over 50 BPF

**Consistency of Cohesive Soils      Blows Per Foot      Approximate Unconfined Compressive Strength**

- Very soft..... 0 to 1 BPF..... < 0.25 tsf
- Soft..... 2 to 4 BPF..... 0.25 to 0.5 tsf
- Medium..... 5 to 8 BPF..... 0.5 to 1 tsf
- Stiff..... 9 to 15 BPF..... 1 to 2 tsf
- Very Stiff..... 16 to 30 BPF..... 2 to 4 tsf
- Hard..... over 30 BPF..... > 4 tsf

**Moisture Content:**

- Dry:** Absence of moisture, dusty, dry to the touch.
- Moist:** Damp but no visible water.
- Wet:** Visible free water, usually soil is below water table.

**Drilling Notes:**

**Blows/N-value:** Blows indicate the driving resistance recorded for each 6-inch interval. The reported N-value is the blows per foot recorded by summing the second and third interval in accordance with the Standard Penetration Test, ASTM D1586.

**Partial Penetration:** If the sampler could not be driven through a full 6-inch interval, the number of blows for that partial penetration is shown as #/x" (i.e. 50/2"). The N-value is reported as "REF" indicating refusal.

**Recovery:** Indicates the inches of sample recovered from the sampled interval. For a standard penetration test, full recovery is 18", and is 24" for a thinwall/shelby tube sample.

**WOH:** Indicates the sampler penetrated soil under weight of hammer and rods alone; driving not required.

**WOR:** Indicates the sampler penetrated soil under weight of rods alone; hammer weight and driving not required.

**Water Level:** Indicates the water level measured by the drillers either while drilling (  $\nabla$  ), at the end of drilling (  $\blacktriangledown$  ), or at some time after drilling (  $\blacktriangledown$  ).

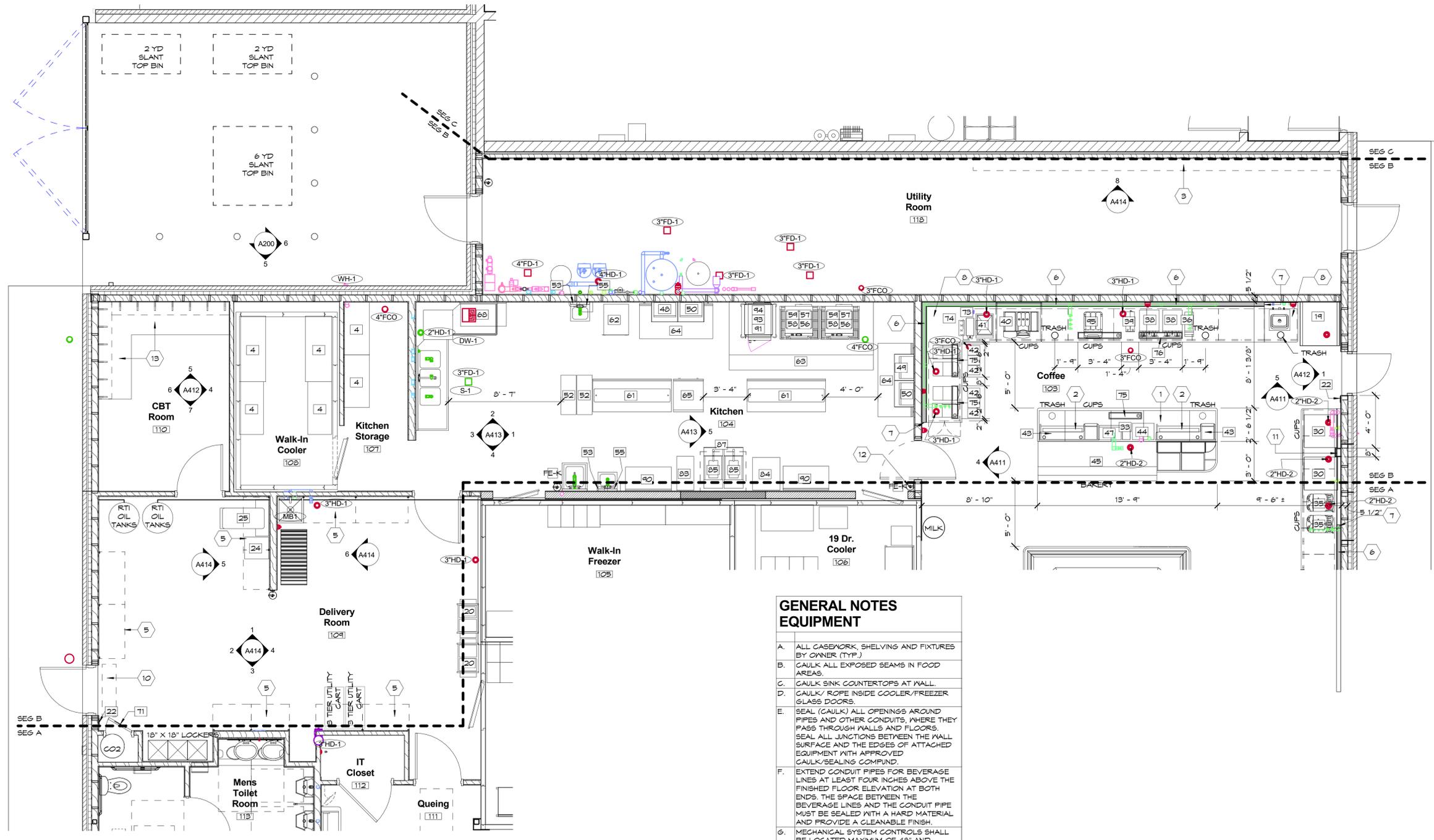


#	TYPE
4	NSF APPROVED SHELVES - COOLERS/FREEZERS
14	ICE CREAM NOVELTY CASE
20	BAG-IN-A-BOX RACK
22	ADA 5'x6' EXIT SIGNAGE WITH BRAILLE SEE SHEET A501 FOR TYP. LOCATION
24	UNDERCOUNTER REFRIGERATOR
25	FLOOR SCRUBBER
30	FOUNTAIN MACHINE
33	HOT WATER DISPENSER
35	SLUSHIE MACHINE
36	SURESHOT FLAVORSHOT 2
38	COFFEE BREWER
39	COFFEE GRINDER
40	XPRESS TOUCH
41	ICE DISPENSER
42	MIX-S MACHINE
43	COFFEE CONDIMENT DISPLAY
44	CREAMER

#	TYPE
45	BAKERY CASE
47	MICROWAVE - SNACK
48	MICROWAVE - KITCHEN
49	SCALE/LABEL MACHINE
50	MONITOR
52	BIN RACK
53	SOAP DISPENSER
55	PAPER TOWEL DISPENSER-KITCHEN
56	TURBOCHEF 13 OVEN RACK
57	TURBOCHEF OVEN
58	TURBOCHEF 13 OVEN
59	CART (STAINLESS STEEL)
61	6'-0" STAINLESS TABLE W/ CENTER SHELF ABOVE
62	BAKERY RACK WITH COVER
63	CAPTIVE ARE HOOD
64	REFRIGERATED PREP TABLE

#	TYPE
65	UNDERCOUNTER FREEZER KITCHEN
68	42" STAINLESS DRY RACK
71	CO2 SIGNAGE BY OWNER
73	COLD BREW DISPENSER
74	COLD BREW CUPS
75	30" DRIP TRAY
76	40" DRIP TRAY
83	GILES DUMP TABLE
84	AYRINK BREADER
85	HENNY PENNY OPEN FRYER
87	3'-6" TYPE 1 HOOD
90	48" STAINLESS TABLE
91	VECTOR OVEN
93	VECTOR STAND
94	HOT FOOD HOLDING CABINETS
95	BEAN-TO-CUP

#	Description
1.	CAFFUCCINO AND BAKERY ISLAND. SEE DETAIL 5/A420
2.	CONDIMENT DISPLAY (TYP. OF 2). SEE DETAIL 8/A420
3.	SHELVES ON MTL. STANDARDS AND BRACKETS (VERIFY MTG. HTG. OF STANDARDS & SHELVES). SEE INTERIOR ELEVATIONS A412.
4.	SNOW MELT
5.	8'-0" STANDARDS AT 4" A.F.F. FOR SHELVING (TYPICAL AT 9 WALLS)
6.	HATCH INDICATES ELECTRICAL RACEWAY.
7.	END OF ELECTRICAL RACEWAY. PROVIDE SOLID SURFACE END CAP.
8.	SOLID SURFACE TOP ONLY.
9.	PROVIDE 8" X 32" OPENING AT 10' - 0" A.F.F. TO 10' - 8" A.F.F.
10.	AIR CURTAIN UNIT ABOVE.
11.	8"X 5"D. BOX FOR POP LINES CHASE. MATERIAL FOR BOX OUT PROVIDED BY OWNER.
12.	10L. CLASS "K" FIRE EXTINGUISHER
13.	32" D. COUNTER MOUNTED 31 3/4" A.F.F. TO TOP



- GENERAL NOTES EQUIPMENT**
- A. ALL CASEWORK, SHELVING AND FIXTURES BY OWNER (TYP.)
  - B. CAULK ALL EXPOSED SEAMS IN FOOD AREAS.
  - C. CAULK SINK COUNTERTOPS AT WALL.
  - D. CAULK/ ROPE INSIDE COOLER/FREEZER GLASS DOORS.
  - E. SEAL (CAULK) ALL OPENINGS AROUND PIPES AND OTHER CONDUITS WHERE THEY PASS THROUGH WALLS AND FLOORS. SEAL ALL JUNCTIONS BETWEEN THE WALL SURFACE AND THE EDGES OF ATTACHED EQUIPMENT WITH APPROVED CAULK/SEALING COMPOUND.
  - F. EXTEND CONDUIT PIPES FOR BEVERAGE LINES AT LEAST FOUR INCHES ABOVE THE FINISHED FLOOR ELEVATION AT BOTH ENDS. THE SPACE BETWEEN THE BEVERAGE LINES AND THE CONDUIT PIPE MUST BE SEALED WITH A HARD MATERIAL AND PROVIDE A CLEANABLE FINISH.
  - G. MECHANICAL SYSTEM CONTROLS SHALL BE LOCATED MAXIMUM OF 48" AND MINIMUM OF 15" ABOVE FINISHED FLOOR.
  - H. DIMENSIONS ARE FROM FINISHED FACE.

**1** EQUIPMENT FLOOR PLAN - SEG B  
1/4" = 1'-0"



KWIK TRIP, Inc.  
P.O. BOX 2107  
1626 OAK STREET  
LA CROSSE, WI 54602-2107  
PH. (608) 781-8988  
FAX (608) 781-8960

PROJECT TITLE: Store #2021 - Gen 3-LH-1Bay-CW  
Kwik Trip, Inc.  
PROJECT LOCATION:

JOB #:	1081---
DATE:	06.26.2020
DRAWN BY:	Author
TYPE:	Permit Set
REVISIONS	
NO.	DATE

SHEET TITLE  
Equipment Floor Plan - Segment 'B'

SHEET NO.  
**A120B**

#	Description
1.	CAFFUCCINO AND BAKERY ISLAND. SEE DETAIL 5/A420
2.	CONDIMENT DISPLAY (TYP. OF 2). SEE DETAIL 5/A420
3.	SHELVES ON MTL. STANDARDS AND BRACKETS (VERIFY MTG. HTG. OF STANDARDS & SHELVES). SEE INTERIOR ELEVATIONS A412.
4.	SNOW MELT
5.	8'-0" STANDARDS AT 4" A.F.F. FOR SHELVING (TYPICAL AT 3 WALLS)
6.	HATCH INDICATES ELECTRICAL RACEWAY.
7.	END OF ELECTRICAL RACEWAY. PROVIDE SOLID SURFACE END CAP.
8.	SOLID SURFACE TOP ONLY.
9.	PROVIDE 8" X 32" OPENING AT 10' - 0" A.F.F. TO 10' - 8" A.F.F.
10.	AIR CURTAIN UNIT ABOVE.
11.	8"X1 x 5"D. BOX FOR POP LINES CHASE. MATERIAL FOR BOX OUT PROVIDED BY OWNER.
12.	10L. GLASS "K" FIRE EXTINGUISHER
13.	32" D. COUNTER MOUNTED 31 3/4" A.F.F. TO TOP

GENERAL NOTES EQUIPMENT	
A.	ALL CASEWORK, SHELVING AND FIXTURES BY OWNER (TYP.)
B.	CAULK ALL EXPOSED SEAMS IN FOOD AREAS.
C.	CAULK SINK COUNTERTOPS AT WALL.
D.	CAULK/ ROPE INSIDE COOLER/FREEZER GLASS DOORS.
E.	SEAL (CAULK) ALL OPENINGS AROUND PIPES AND OTHER CONDUITS, WHERE THEY PASS THROUGH WALLS AND FLOORS. SEAL ALL JUNCTIONS BETWEEN THE WALL SURFACE AND THE EDGES OF ATTACHED EQUIPMENT WITH APPROVED CAULK/SEALING COMPOUND.
F.	EXTEND CONDUIT PIPES FOR BEVERAGE LINES AT LEAST FOUR INCHES ABOVE THE FINISHED FLOOR ELEVATION AT BOTH ENDS. THE SPACE BETWEEN THE BEVERAGE LINES AND THE CONDUIT PIPE MUST BE SEALED WITH A HARD MATERIAL AND PROVIDE A CLEANABLE FINISH.
G.	MECHANICAL SYSTEM CONTROLS SHALL BE LOCATED MAXIMUM OF 48" AND MINIMUM OF 15" ABOVE FINISHED FLOOR.
H.	DIMENSIONS ARE FROM FINISHED FACE.

CAR WASH EQUIPMENT SCHEDULE	
TAG	DESCRIPTION
A	AIR COMPRESSOR
B	WATER SOFTENER
BU	BOILER UNIT
C	REVERSE OSMOSIS UNIT
CPO	CPO CONTROLLER
CA	CHOICENASH XT GANTRY
CWB**	*CITY WATER BOOST PUMP
D	RO WATER PUMP
DC	DOOR CONTROL - MARK VII
DE	DOOR SENSOR EYE
DR	DRYER CONTROL BOX (40hp)
DT	DRYER COUNTDOWN TIMER
E	SPOTFREE STORAGE TANK
EC	ENERGY CHAIN FEED SYSTEM
EP	ELECTRICAL PANELS (BY OTHERS)
F	BRINE TANK
FD	FS-40D FREESTANDING DRYER
FL	FLOOR LOOP SENSORS
G	RO REJECT WATER PUMP
GD	GARAGE DOORS
GR	GUIDE RAILS, SST LOW PROFILE
H	CARBON FILTER
JB	IN BAY JUNCTION BOX
K	AIR DOOR CONTROL BOX
L	CPC PANEL
MB	MENU BOARD - T POSITION
MD	MAIN DISCONNECT
P	DOOR CONTROL BOX
PP	PUMPING PLANT/ STAND
SSB	STOP-GO-BACK UP SIGN
SS	SALT STORAGE
T	COIN/ CODE BOX, POINT OF SALE
TK	GANTRY TRACK 33', LOW PROFILE
TR	TREADLE SENSOR PAD
UC	UNDERCARRIAGE WASH MANIFOLD W/ FLAT COVER
WST	WASH STAT

**VANMAGGE**  
ARCHITECTS INC

750 N. Third Street  
Ph (608) 784-2729

La Crosse, WI 54601  
Fax (608) 784-2826

**Kwik Trip**

**Kwik Star**

Kwik Trip, Inc.  
P.O. BOX 2107  
1628 OAK STREET  
LA CROSSE, WI 54602-2107  
PH. (608) 781-8988  
FAX (608) 781-8960

PROJECT TITLE:  
Store #2021 - Gen 3-LH-1Bay-CW

PROJECT LOCATION:  
Kwik Trip, Inc.

JOB #: 1081-...

DATE: 06.26.2020

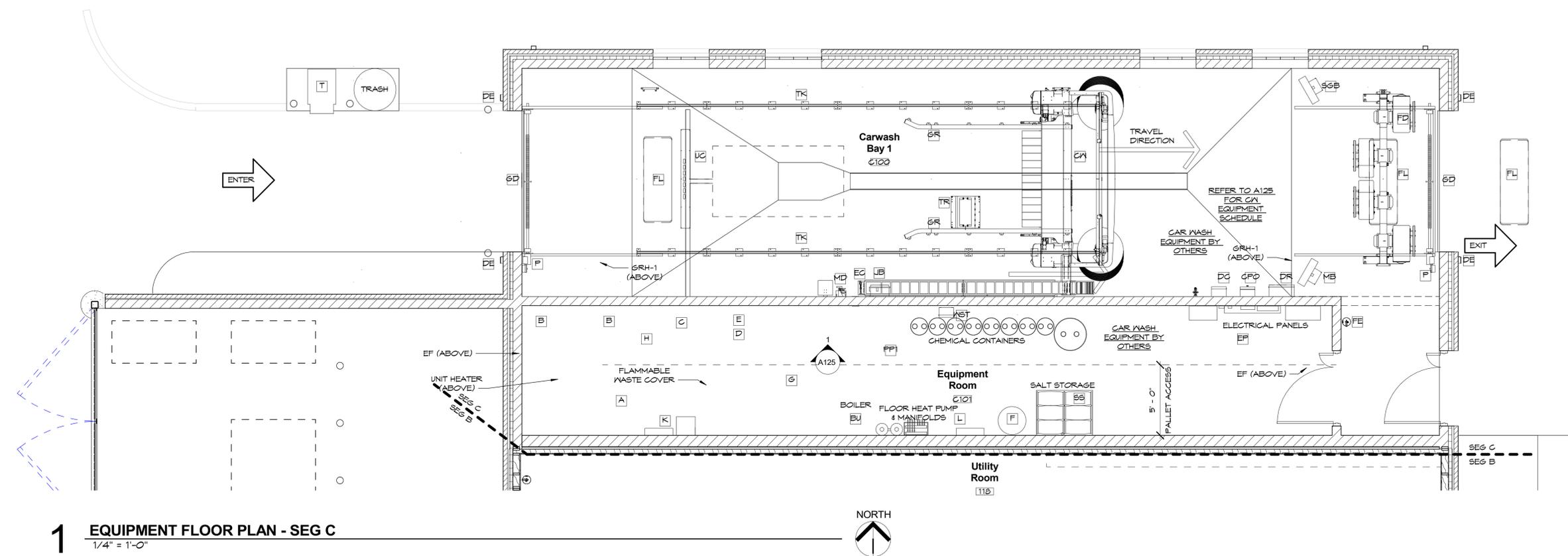
DRAWN BY: Author

TYPE: Permit Set

REVISIONS NO.	DATE

SHEET TITLE  
Equipment Floor Plan - Segment 'C'

SHEET NO.  
**A120C**



**1** EQUIPMENT FLOOR PLAN - SEG C  
1/4" = 1'-0"



Real People. Real Solutions.

12224 Nicollet Avenue  
Burnsville, MN 55337-1649

Ph: (952) 890-0509  
Fax: (952) 890-8065  
Bolton-Menk.com

## MEMORANDUM

**Date:** July 17, 2020  
**To:** Renee Christianson, Community Development Coordinator  
**From:** Rich Revering, PE – City Engineer  
**Subject:** Review of Kwik Trip Convenience Store #1116  
Elko New Market  
Project No.: T17.122238

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### BACKGROUND

The City Engineer's Office was asked to review the above-referenced plan set on June 30, 2020. We offer the following comments:

#### Erosion Control

1. Sheet SP3: Install erosion control BMPs adjacent to the proposed storm sewer line running south of the property that ties into the existing culvert.
2. Sheet SP5: Provide a trained individual with a current certification in the SWPPP documentation. The one shown expired on May 31, 2020.

#### Streets

3. Increase the length of the north side radius at the south entrance on France Avenue so the semi wheels do not run over the curb.

#### Utilities

4. Provide hydrants in conformance with the Fire Code or verify with Fire Chief that hydrants at Marketplace Boulevard and easterly side of France Avenue are acceptable for department staging in the event of a fire. Serve hydrants with 8-inch pipe.
5. Sanitary sewer connections to pipes are typically preferred over connections to manholes to avoid mainline maintenance interference and minimize infiltration. Confirm connection type allowed w/ Public Works Superintendent.

#### Storm Sewer

6. Sheet SP4.1: Revise the west invert for CBMH 107, it is shown incorrectly.
7. Sheet SP4.1: The invert of proposed STMH 101 should be shown lower than the existing FES elevation of 1125.16.

End of Memo



601 Main Street  
Elko New Market, MN 55054  
phone: 952-461-2777 fax: 952-461-2782

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## MEMORANDUM

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**TO:** PLANNING COMMISSION & CITY COUNCIL  
**FROM:** HALEY SEVENING, PLANNER I  
RENEE CHRISTIANSON, COMMUNITY DEVELOPMENT SPECIALIST  
**RE:** ACCESSORY DWELLING UNITS  
**DATE:** JULY 28, 2020

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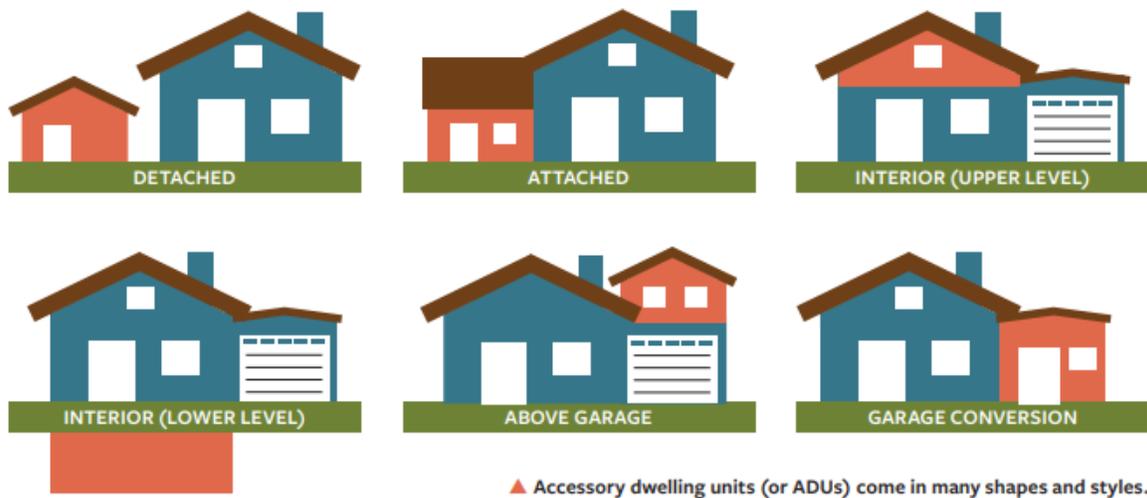
### **Background / History**

On May 26<sup>th</sup>, 2020, Staff presented information on housing to the Planning Commission. A portion of the presentation focused on an Affordable Housing Toolbox, which included different policies, programs, and/or ordinances that could be utilized to foster affordable housing. Following the presentation, the Planning Commission made a recommendation to the City Council that all tools be further explored. On June 25<sup>th</sup>, 2020, Staff presented the same information to the City Council. The City Council concurred with the Planning Commission and directed Staff to further explore the following affordable housing tools:

- Removal/Reduction of Regulatory Costs
- 4D Tax Program
- Fee Waiver Program / Fee Equity Analysis
- Accessory Dwelling Units
- Inclusionary Housing
- Preservation of Existing Housing

The first tool Staff is presenting to the Planning Commission and City Council is adoption of an Accessory Dwelling Unit (ADU) Ordinance. ADUs are small, self-contained living units that have their own kitchen, bedroom(s), bathroom space, and, depending on the type, entrance. Often called granny flats, mother-in-law suites, or secondary dwelling units, ADUs are apartments that can be located within the walls of an existing or newly constructed single-family home (known as internal or interior ADUs), an addition to an existing home (known as attached ADUs), or freestanding structures on the same lot as the principal dwelling unit (known as detached ADUs). More general information regarding ADUs is included in the attached Family Housing Fund ADU Policy Brief and Strong Towns ADU Article.

ADUs have been recognized around the country as a strategy to increase the amount of affordable housing in a community and assist homeowners with mortgage and ownership costs without requiring City funding. Elko New Market does not currently permit ADUs, yet many metro area cities do.



**Benefits, Costs, & Concerns**

There are several benefits that make ADUs an attractive option for Elko New Market and property owners. Allowing ADUs has the potential to:

- Expand housing options for current and future residents (i.e. increase in rental units, life cycle/aging in place housing, or guest/independent living space for family/friends).
- Increase housing inventory without expensive land acquisition costs (more units at a more affordable price).
- Increase the customer base to support existing and attract additional local businesses.
- Provide supplementary income for property owner.
- Increase property values.

Costs associated with ADUs include:

- Cost of construction (can range anywhere from \$50,000 to \$200,000 depending on the type of ADU constructed).
- Utility and maintenance costs for ADU.
- Increased property taxes.

According to the American Planning Association, public resistance to ADUs usually takes the form of a perceived concern that they might transform the character of the neighborhood, increase density, add to traffic, make parking on the street more difficult, increase school enrollment, and put additional pressure on fire and police service, parks, or water and wastewater. However, communities that have allowed ADUs find that these perceived fears are mostly unfounded or overstated when ADUs are actually built.

**Research & Insights from Area Cities**

Staff has researched ADU ordinances in 18 suburban cities in the Twin Cities area. The 18 cities included: Apple Valley, Belle Plaine, Bloomington, Burnsville, Crystal, Eagan, Faribault, Inver Grove Heights, Lakeville, Long Lake, Mahtomedi, Minnetonka, Plymouth, Prior Lake, Richfield, Roseville, Shoreview, and White Bear Lake. Within these cities, the most common reasons cited for allowing ADUs are to:

- Create new housing opportunities and choices while respecting the look and scale of single-dwelling development.

- Support efficient use of existing housing stock and infrastructure.
- Provide housing that responds to changing family needs.
- Provide affordable housing options.

A summary of the ADU ordinances from each of the 18 cities is included as an attachment. The research summary reviews common performance standards that should be considered when drafting an ADU ordinance. Table 1 below also highlights the most common performance standard from the 18 cities. If the Planning Commission and City Council are supportive of an ADU ordinance, performance standards must be carefully considered to avoid overregulation. Overregulation has the potential to increase the cost or preclude many properties from being able to construct ADUs.

**Table 1: Most Common Performance Standard for ADUs**

Performance Standards	Most Common Standard
Type	Attached, internal, or detached
Approval Method	Permitted by Right
Size Restrictions	Min: 300 SF, Max: Varies, but most often SF limit and % limit
Number of Bedrooms	Not regulated
Maximum Occupancy	Not regulated
Minimum Lot Size	Must meet zoning district standards
Setbacks	Subject to existing zoning standards
Where Permitted	Single family lots only
Utility Service	Same service as principal unit
Parking Requirements	Two off-street parking spaces required
Addressing	Separate entrance, separate address
Owner Occupancy	Owner occupancy in one of the units
Design Requirements	Consistent with principal unit
Home Occupations	Not regulated

Staff also reached out to each of the Cities to get further insights and recommendations regarding ADUs. Table 2 below includes comments received from their Staff.

**Table 2: Comments Received from Other Cities**

Performance Standard	Most Common Standard
Belle Plaine	We have not had anyone step forward for an IUP for one. The ordinance was placed into effect in Aug of last year (2019). I'd say demand is not there at this time. We do try to mention to developers and builders as educational outreach. I'm glad we have the option though we are

	probably too restrictive requiring an IUP.
<b>Bloomington</b>	The most common ADU we get requests for is a tiny house or separate unit on the lot. We also found some who had been interested were landlords looking to capitalize on the second unit. So we have home owner occupancy and rental license requirements as well.
<b>Eagan</b>	We get more questions about Tiny Homes, which often don't or can't meet ADU requirements.
<b>Inver Grove Heights</b>	Overall, the response allowing ADU's has been positive. The main issue we run into are the size maximums regarding ADU's in detached accessory structures. For example, the footprint of the building is 1,000 square feet which complies with our code; once they add the upper level for the ADU they now exceed the maximum size of accessory building and would require a variance or a smaller footprint.
<b>Lakeville</b>	Most ADU permits have been issued in new developments.
<b>Mahtomedi</b>	Using the CUP process to permit them has worked well. It is nice to be able to look at the property on an individual level to see if it can accommodate a second dwelling and to also have a chance for neighbors to provide input.
<b>Minnetonka</b>	We are beginning the process of considering detached ADUs. Detached ADUs were not originally allowed in the adopted ordinance because there were concerns over the visual/massing of having multiple homes on a single-family property.
<b>Richfield</b>	Our code requires that ADU utilities be totally separate from the primary residence, even if it's an attached ADU. I've heard that this can sometimes dissuade people from building an ADU.
<b>Roseville</b>	As more people have recently begun inquiring about ADUs over detached garages we've discovered that even though the zoning code says it's allowed, the height limit for detached garages effectively prohibits the height needed to locate an ADU above a detached garage. We have an occupancy permit requirement that serves two purposes: it allows us to ensure that proposed ADUs conform to the applicable standards, and it requires us to inform nearby homeowners of an application. Our expectation, frankly, was that informing nearby homeowners of a neighbor's intention to build an ADU would basically be an invitation to oppose the permit or appeal its issuance. To our surprise, if those notified homeowners even bother to contact us (as the notification invites them to do), the vast majority of them are simply inquiring about their own ability to add an ADU. So far, I think the only reason we haven't seen more ADUs is that the substantial cost to build them has remained greater than the potential benefit.
<b>White Bear Lake</b>	Resident concerns about traffic or use as short-term rental have not been

realized. Currently considering changing CUP requirement to allowing ADUs by right. No notable issues or concerns with existing ADUs.

Finally, Staff reached out to four Scott County cities (Jordan, New Prague, Savage, and Shakopee) that do not currently allow ADUs to better understand why they don't allow them. The most cited reasons for not allowing ADUs are:

- Concerns regarding use of ADU as a rental, especially by non-family members.
- Concerns regarding the scale, mass, and/or visual impacts of ADUs within established single-family neighborhoods.
- No push or interest, by either elected officials or residents, to consider ADUs.

### **Why Should Elko New Market Consider ADUs?**

ADUs have the potential to meet the following goals and policies identified in the current and draft Comprehensive Plans:

#### 2030 Comprehensive Plan Goals

- Allow for the potential development of a variety of dwelling unit types, styles, and choices to meet the changing life cycle needs for a wide spectrum of people with a variety of income levels.
- Take a more proactive stance on developing provisions to allow for housing to meet the needs of a variety of people.
- Promote new housing which will fit within the character of the existing community.

#### Draft 2040 Comprehensive Plan

- Goals
  - Provide housing options for people in all life stages and of all economic means.
  - Remove barriers to providing a variety of housing options.
  - Provide opportunities for the City's share of affordable dwelling units.
- Policies
  - Maintain zoning and subdivision regulations that allow for the construction of a variety of housing types and price ranges.
  - Evaluate current City ordinances which may provide barriers to providing housing options and make changes where necessary.
  - Provide programs and incentives to encourage the development of affordable dwelling units.

In addition to the goals and policies identified in the current and draft Comprehensive Plans, the current market indicates that affordable housing will not be constructed without some sort of intervention or subsidy. The City must proactively work to foster affordable housing in order to meet the affordable housing needs projected by both the Scott County Community Development Agency (CDA) and the Metropolitan Council. The projected affordable housing needs by both organizations are as follows:

- Scott County CDA → 208 affordable units by 2040
- Metropolitan Council → 326 affordable units by 2030

Currently, much of the demand for constructing these affordable units is being placed on developers in the private sector. However, allowing ADUs in Elko New Market presents an opportunity for individual homeowners to contribute to the affordable housing needs while reducing the burden placed on developers.

Finally, it should be noted that adoption of an ADU ordinance will likely not result in the construction of a large number of ADUs that completely transform the nature and character of single-family neighborhoods. Staff from some of the cities that were researched indicated that very few ADUs have actually been built following adoption of an ADU ordinance.

For example, the City of Bloomington, which has had an ADU ordinance in place since 2009 has seen only one ADU be constructed. In Belle Plaine, the city that most recently adopted an ADU ordinance (in 2019), no ADUs have been constructed. Based on information provided, the city seeing the highest level of ADU construction is Minnetonka, with approximately three to six new ADUs per year.

### **Other Considerations**

If the Planning Commission and City Council are supportive of adopting an ADU ordinance, there are several related items that the City may want to consider:

- Ordinance amendments related to the Detached Accessory Structure Ordinance.
- How preexisting, nonconforming ADUs will be handled.
- Whether or not the City should adopt a rental registration policy for rental ADUs and other rental properties.
- Whether or not the City should adopt a Short-Term Rental Ordinance.

These items are being presented for informational purposes only. Further discussion would occur at future meetings if ADUs are supported.

### **Requested Action**

At this time Staff is seeking preliminary feedback from the Planning Commission and City Council on the topic of ADUs. Specific questions for the Planning Commission and City Council are included below and additional input is welcome. If supported, Staff would do any additional research necessary and bring back a draft ADU ordinance for review by the Planning Commission at a future meeting.

- Does the Planning Commission support the development of a regulatory framework allowing ADUs?
  - If so, what is the intent or purpose behind allowing ADUs?
  - Do you have any concerns related to allowing ADUs?
  - Initial thoughts on types of ADUs to be permitted and performance standards?
    - Type
    - Approval Method
    - Size Restrictions
    - Number of Bedrooms
    - Maximum Occupancy
    - Minimum Lot Size
    - Setbacks
    - Where Permitted
    - Utility Service
    - Parking Requirements
    - Addressing
    - Owner Occupancy

- Design Requirements
- Home Occupations

Attachments

- Family Housing Fund ADU Policy Brief
- Strong Towns ADU Article
- Research Summary of ADU Ordinances

POLICY BRIEF

# *ADUs: Housing Options for a Growing Region*

## **Accessory Dwelling Units (ADUs)**

are a flexible, neighborhood-scale solution to regional housing needs. Cities can encourage ADUs as part of their overall housing strategy by adopting proven policies.



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## What is an ADU?

ADUs are often known as “carriage houses,” “in-law suites,” and “granny flats.” They are adjacent or attached to a primary home, and have their own entrance, kitchen, living area, and bathroom. ADUs can be located within a home, attached to a home, or as a detached structure in a backyard (sometimes above a garage). ADUs are a unique housing form created by individual homeowners and scattered throughout neighborhoods.



## What makes ADUs good for cities?

*ADUs provide **affordable options in the private market**: Most ADU rents are affordable to a household earning less than \$56,000 annually.<sup>1</sup>*

- ADUs represent **gentle, or “hidden” density** as a form of small-scale infill housing.
- ADUs provide **access for renters** to established, well-connected neighborhoods.
- ADUs are built by homeowners on **existing lots**, providing new housing without expensive land acquisition costs, and add **value** to the property, which can lead to increased property tax revenue for a city.
- ADUs typically serve **one- and two-person households**, a growing demographic segment which comprises the majority of Twin Cities households.<sup>2</sup>
- ADUs support stable homeownership by serving **lifecycle housing** needs. Over time, rental income provided by an ADU can help homeowners pay their mortgages or save up. Homeowners may use their ADU to house family members who need care, or they may move into the ADU themselves to downsize.
- ADUs are **environmentally-friendly** housing options because they are smaller and use less energy than the average home. They help reduce transportation-related environmental impacts when they are located near employment centers and established public transit routes.<sup>3</sup>
- ADUs support the **local economy**, as homeowners typically hire local construction and design firms to build them.
- ADUs help create **vibrant neighborhoods** as new residents increase the customer base for nearby businesses and services.

<sup>1</sup> Rent data from FHFund survey results. See also: Garcia, David. UC Berkeley Turner Center for Housing Innovation. Dec. 2017. ADU Update: Early Lessons and Impacts of California’s State and Local Policy Changes. [turnercenter.berkeley.edu/uploads/ADU\\_Update\\_Brief\\_December\\_2017\\_.pdf](https://turnercenter.berkeley.edu/uploads/ADU_Update_Brief_December_2017_.pdf)

<sup>2</sup> Metropolitan Council. Thrive MSP 2040 Housing Policy Plan. July 2015. p. 9

<sup>3</sup> See Stephan, A., Crawford, R.H., 2016. The relationship between house size and life cycle energy demand: implications for energy efficiency regulations for buildings. *Energy* 116 (Part 1), 1158–1171. [dx.doi.org/10.1016/j.energy.2016.10.038](https://dx.doi.org/10.1016/j.energy.2016.10.038) A 2014 study found that ADU residents in Portland were less likely than the average to own cars (State of Oregon Department of Environmental Quality, 2014. Accessory dwelling units in Portland, Oregon: evaluation and interpretation of a survey of ADU

## Hidden Density

Can you spot the modern, spacious ADU behind this home in Saint Paul?

Eric & Chrissi  
Larsen inside  
their ADU

### Why do we need ADUs as a housing option?

ADUs can help reduce pressures on the regional housing market, including:

- **Increasing demand** for more housing units: To meet the needs of anticipated workforce growth and other population trends, the seven-county Twin Cities region needs to add nearly 13,000 units of housing each year through 2040.<sup>4</sup> ADUs engage private homeowners as a new set of partners addressing this housing need, without public subsidy.
- **Low supply** of rental housing stock: Throughout the Twin Cities metro area, vacancy rates for studio and one-bedroom units are 2.1% and 2.3%, respectively—far below a healthy rate of 5% or more.<sup>5</sup> Adding ADUs in existing neighborhoods helps to address this gap.
- **Cost** pressures for renters: The greatest demand over the next 20 years will be for rental units priced below \$1,875/month (in 2019 dollars), as ADUs typically are.<sup>6</sup>
- **Smaller households:** The type of new housing needed in the coming decades will be affected by changing demographic trends. Nearly half of the region's projected household growth will be individuals living alone, and ADUs are typically designed for these smaller households.<sup>7</sup>
- **Aging population:** Four-fifths of household growth will be in older households headed by individuals aged 65 and older, many seeking options to downsize in their own neighborhoods; ADUs provide this option.<sup>8</sup>



### Where can I find ADUs?

Currently, an estimated 18 cities in the Twin Cities metropolitan area have policies permitting ADUs and approximately 150 permitted ADUs exist in the region. However, ADUs have always existed in the Twin Cities.

### How much housing could ADUs provide?

**ADUs are a cost-effective way to meet a substantial portion of the Twin Cities region's future housing need without public subsidy.**

If ADUs in the seven-county metro area became as common as they are in Portland, Oregon (representing about 1.5% of single-family homes), we could create 11,000 new housing units, potentially meeting most of the region's need for new housing for households who earn \$40,000-50,000 per year.<sup>9</sup>

 **11,000**  
NEW HOUSING UNITS

<sup>4</sup> Met Council Housing Policy Plan, p. 9

<sup>5</sup> Marquette Advisors Apartment Trends, 1st Quarter 2018.

<sup>6</sup> FHFund/Lisa Sturtevant & Associates, 2018, forthcoming

<sup>7</sup> Met Council Housing Policy Plan, p. 9

<sup>8</sup> Met Council Housing Policy Plan, p. 9

<sup>9</sup> Met Council Housing Policy Plan, p. 151: The region needs 9,550 new units of housing to meet the needs of households earning between 51-80% of the Area Median Income by 2030. Per Metropolitan Council 2017 estimates, there are approximately 721,035 single-family homes in the Twin Cities region. stats.metc.state.mn.us/profile/detail.aspx?c=R11000

# Encouraging ADUs: Best Practices for Cities

## REMOVE REGULATORY BARRIERS

- Allow different types of ADUs as an accessory to all single-family or small multifamily homes, permitted by right rather than conditionally
- Designate ADU experts within departments to facilitate a clear permitting process
- Remove or reduce parking minimums
- Remove owner-occupancy restrictions
- Make design standards more flexible

## LOWER COSTS AND INCREASE ACCESS TO CAPITAL

- Offer homeowners waivers, discounts, tiered pricing, and payment plans for fees
- Develop an ADU loan program for homeowners
- Work with developers to incentivize building ADUs in new construction

## PROMOTE ADUs AND INCREASE ACCESS TO INFORMATION

- Create a dedicated webpage and resource materials for ADU development
- Host quarterly informational workshops about ADUs
- Sponsor, promote, and participate in ADU tours



*Flexible for  
Multigenerational  
Households*



Fue Lee's parents live in the first-floor

ADU attached to the family's house in North Minneapolis. Fue and his adult siblings live in the main home.

*The Lees' home and its neighbor to the north were developed by the City of Lakes Community Land Trust as the first two homes to be built with an ADU in Minneapolis.*

## How can local policies support ADU development?

The evidence is clear: local government policies and practices that reduce regulatory and cost burdens make a critical difference in whether ADUs can reach their full potential for communities.

- In **Austin, Texas**, allowing larger ADUs (up to 1,100 square feet) and reducing other requirements<sup>10</sup> paved the way for permit requests to rise nearly tenfold.
- In **Portland, Oregon**, annual ADU permit volume increased from just 24 (in 2009) to 615 (in 2016) when it waived development fees for ADUs, saving homeowners \$8,000-12,000 per unit.<sup>11</sup>
- In **Los Angeles, California**, ADU permits jumped from 80-90 per year to 1,980 in 2017, after California's state legislature required cities to adopt ADU policies.<sup>12</sup>



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<sup>10</sup> Austin Development Services Department. Accessory Dwelling Units. [www.austintexas.gov/page/adu](http://www.austintexas.gov/page/adu)

<sup>11</sup> City of Portland, Oregon. City Council Extends the SDC Waiver for ADUs, with Conditions. June 27, 2018. <https://www.portlandoregon.gov/bds/article/689356>

<sup>12</sup> Garcia, David. UC Berkeley Terner Center for Housing Innovation. Dec. 2017. ADU Update: Early Lessons and Impacts of California's State and Local Policy Changes. [ternercenter.berkeley.edu/uploads/ADU\\_Update\\_Brief\\_December\\_2017\\_.pdf](http://ternercenter.berkeley.edu/uploads/ADU_Update_Brief_December_2017_.pdf)

## Encouraging ADUs in Your City

### REMOVE REGULATORY BARRIERS

**Allow ADUs to be permitted by right for all single-family and two-family developments.**

In California, allowing ADUs to be processed ministerially (i.e. administratively/by right) has had a major impact, with a rapid rise in ADU permit applications after the enactment of SB 1069 and AB 2299 in January 2017.<sup>13</sup> The City of Oakland had a sevenfold increase, from 33 permit applications in 2015 to 247 in 2017. The City of Los Angeles had a nearly 25-fold increase in applications, from 80 in 2016 to 1,980 in 2017.

**Remove or reduce parking minimums.**

In Oregon, a survey conducted by the Department of Environmental Quality found that ADUs had a negligible impact on parking congestion.<sup>14</sup> ADU residents had a below-average vehicle ownership rate (less than one per household), and the dispersed nature of ADU development meant any additional on-street parking impact was also dispersed throughout the city.

**Remove owner-occupancy and household size restrictions.**

Most peer cities nationally and three Minnesota cities (Crystal, Stillwater, and Northfield<sup>15</sup>) do not have owner-occupancy requirements. These requirements limit the use of the property over time and may be a disincentive to homeowners considering ADU development or limit their financing options.

**Make design standards more flexible.**

After adjusting its regulations in 2015 to allow larger floor areas, Austin, Texas saw a marked increase in ADU development, from 250 issued permits from 1994 to 2015 to more than 600 in the three years since the change.<sup>16</sup>

**Designate ADU experts in departments to facilitate a clear permitting process.**

To clear its backlog of ADU applications, San Francisco is working with multiple city departments to define a checklist of consistent guidelines to help homeowners successfully navigate city processes.<sup>17</sup>

### LOWER COSTS AND INCREASE ACCESS TO CAPITAL

**Offer waivers, discounts, tiered pricing, and payment plans for fees.**

**WAIVER FOR AFFORDABILITY:** The City of Santa Cruz, California waives permit fees on a sliding scale in exchange for a commitment to renting an ADU to a low-income household. Approximately 39 households have used this waiver since 2016.<sup>18</sup>

**TIERED PRICING:** Most cities already offer tiered pricing in some form, such as for building permits. Offering tiered pricing for other fees, such as sewer access charges, can help reduce what would otherwise be a larger fixed cost for homeowners wishing to build an ADU.

**Develop an ADU loan program.**

The County of Santa Cruz in California,<sup>19</sup> the City of Portland, Oregon,<sup>20</sup> and the West Denver Renaissance Collaborative (WDRC) in Colorado<sup>21</sup> are developing low- or no-interest loan programs for ADU development. Each program has an affordability focus, either creating affordable rental units or building wealth and stability for lower-income homeowners.

Santa Cruz County also has a specialized *My House, My Home ADU* loan program to help low-income senior homeowners build ADUs so that they can afford to age in place.<sup>19</sup>

**Work with developers to incentivize building ADUs in new construction.**

The City of Lakes Community Land Trust (CLCLT) builds and sells multigenerational-living homes with attached ADUs in Minneapolis. The homes are designed for flexibility, with ADUs at the back of the first floor that can open to the inside of the main home or can be accessed through a separate entrance, allowing the home to meet changing housing needs over multiple generations.

*Continued  
on next page*

## APPENDIX BEST PRACTICES FROM PEER CITIES

...continued

### PROMOTE ADUs AND INCREASE ACCESS TO INFORMATION

**Create a dedicated webpage and host informational workshops on ADU development.**

City of Santa Cruz, California has become a national model by appointing dedicated staff to the development of its ADU program, creating a guide for homeowners, sharing prototypes of architectural plans, hosting workshops, and creating a webpage with ADU information.<sup>22</sup>

**Sponsor, promote, and participate in ADU tours.**

The city of Portland boasts an annual ADU tour, run in partnership between advocates and the City. It has been a successful beginning point for many ADU homeowners, who embarked on their developments after attending the tour.<sup>23</sup> Locally, a few ADUs already have been popular stops on the Minneapolis & Saint Paul Home Tour.<sup>24</sup>

<sup>13</sup> Garcia, David.

<sup>14</sup> Oregon Department of Environmental Quality.

<sup>15</sup> Northfield is noted here but is not counted as part of the 18 cities that have an ADU policy as it is outside the Twin Cities metro area.

<sup>16</sup> City of Austin. Open Data. data.austintexas.gov

<sup>17</sup> Sabatini, Joshua. "Just 23 in-Law Units Built after Two Years as SF Seeks to Iron out Approval Process." The San Francisco Examiner. 25 Feb. 2018. [www.sfexaminer.com/just-23-law-units-built-two-years-sf-seeks-iron-approval-process](http://www.sfexaminer.com/just-23-law-units-built-two-years-sf-seeks-iron-approval-process)

<sup>18</sup> City of Santa Cruz 2016 Accessory Dwelling Units Fee Waiver Information and Application [www.cityofsantacruz.com/home/showdocument?id=53802](http://www.cityofsantacruz.com/home/showdocument?id=53802)

<sup>19</sup> Santa Cruz County. Accessory Dwelling Units. Plan Your Financing. [www.sccoplaning.com/ADU/Planyourfinancing.aspx](http://www.sccoplaning.com/ADU/Planyourfinancing.aspx)

<sup>20</sup> Portland Housing Bureau. Accessory Dwelling Unit Pilot Loan Program. [www.portlandoregon.gov/phb/article/661992](http://www.portlandoregon.gov/phb/article/661992) [PDF Slides]

<sup>21</sup> Martinez-Stone, Renee. 28 June 2018.

<sup>22</sup> Peterson, Kol. 2018. Backdoor Revolution: The Definitive Guide to ADU Development. Accessory Dwelling Strategies, LLC. p. 227-228

<sup>23</sup> Eastman, Janet. 29 Aug. 2017. "Get inside 24 Rentable Granny Flats: Portland's Accessory Dwelling Unit Tour (Photos)." OregonLive.com. [www.oregonlive.com/hg/index.ssf/2017/08/granny\\_flat\\_adu\\_tour\\_pdx\\_kol\\_p.html](http://www.oregonlive.com/hg/index.ssf/2017/08/granny_flat_adu_tour_pdx_kol_p.html).

<sup>24</sup> Lee, Chris. 25 Apr. 2018. "Minneapolis & Saint Paul Home Tour Features Homes and Neighborhood Pride." Midwest Home. [midwesthome.com/124463-2](http://midwesthome.com/124463-2)



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# STRONG TOWNS

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## If You're Going to Allow ADUs, Don't Make It So Hard to Build One

[Daniel Herriges](#) - September 11, 2018

Granny flats, garage apartments, mother-in-law suites: call them what you like. Accessory Dwelling Units (ADUs) are beloved in the planning profession and among affordable housing advocates, and for good reason. These modest homes, often the size of a studio apartment, can be found in the backyard of a larger house, attached to or above the garage, or attached to another home but with a separate entrance. They are a wonderful way to make it possible for more people to live in in-demand neighborhoods without dramatically changing those neighborhoods' visual character.

Whereas new apartment buildings or other larger structures can be met with fierce resistance from nearby homeowners, ADUs ought to, in theory, be more palatable to these residents than high-density infill in their neighborhoods. The reason is simple: a street lined with attractive single-family homes with ADUs half-hiding in the backyards still looks and feels and operates a lot like, well, a street lined with attractive single-family homes.

ADUs are a way to gently and incrementally make a neighborhood less exclusionary. They are a relatively affordable rental option for those who do not wish to or have the resources to become homeowners. They are a valuable source of income for their landlords, who are usually established residents, as opposed to developers who may have no ties to the neighborhood or even the city. ADUs add pedestrians to the sidewalks, customers to local businesses, and dollars to the tax base that pays for city services. And they do all of this while making more efficient use of infrastructure that already exists.

ADUs are a quintessentially Strong Towns approach to urban growth and affordability issues: bottom-up, decentralized, incremental, scalable and adaptable. They exemplify the principle of steady, distributed neighborhood change as the antidote to sudden, disruptive neighborhood change.

And yet, in practice, very few cities have seen a sizable number of new ADUs built in recent years. Most existing ones still date to eras like the 1920s, in which urban neighborhoods were still designed for walkability and without such restrictions as parking requirements, strict maximum densities and minimum lot sizes.

A key reason that ADUs aren't spreading like wildfire, even when affordable-housing concerns are, is that most recent efforts to allow ADUs come with a long list of stipulations

whose end result is to make building one into a project that's prohibitively expensive, complicated, and/or risky for all but a few homeowners.

An ordinance on the table in Raleigh, North Carolina is a great example of this. [The Raleigh News-Observer's Anna Johnson explains:](#)

*The recommended rules outline a special district that would allow backyard cottages, but only after a resident applies for it and a majority of 10 acres worth of the applicant's neighbors agree to the district. The recommendation also outlines how close the backyard cottage can be to other properties and how big it can be. It also regulates lighting and parking.*

*The rules wouldn't allow the cottages to be used for short-term rentals like AirBNB and would limit the occupancy to two adults.*

Unfortunately, Raleigh is the norm rather than the exception when it comes to ADU rules. Most cities that have nominally allowed ADUs have saddled them with so many restrictions that nearly everyone who might actually put one on their lot is deterred from doing so. ADU researcher Martin John Brown [runs through a litany of fine-print obstacles:](#)

- *ADUs not allowed "as of right": when a homeowner must go through a special "conditional use" or "discretionary action" process before their ADU can be permitted, it makes the whole ADU development project more of a gamble.*
- *Minimum lot size required: ADUs are often prohibited on smaller lots.*
- *Offstreet parking required for an ADU: in places with small lots, it may be impossible to find the room to place new dedicated parking spaces. Meanwhile, there is **no evidence that ADUs contribute to neighborhood parking problems.***
- *Owner occupancy is required on the property: this requirement reduces the flexibility of future uses of the property, which may be a discouragement to development in the first place.*
- *ADUs must meet affordable housing terms (rare).*
- *Permits and systems development charges (SDCs) are extremely expensive.*
- *Design constraints: common and widely accepted conditions about ADUs concern their maximum area and height, their distance from property lines, their style in relation to the primary dwelling, etc. These may act as barriers for some homeowners.*

We can observe something like a controlled experiment in ADU building by looking at Portland, Oregon and Vancouver, British Columbia: two cities which (despite the international border and a few hundred miles separating them) have similar climates, similar built environments, similar cultures of environmentalism and left-leaning politics, and similar housing affordability challenges. In a CityLab article from 2017 titled "[How Cities Get Granny Flats Wrong](#)," Bryn Davidson answers her title's question by means of photographic negative: explaining what Vancouver actually got *right*. Starting in 2009, the city allowed ADUs nearly citywide, on what totaled more than 65,000 lots. No additional parking was required, no public hearing or approval from neighbors, and no expensive or

cumbersome design requirements. You can build an ADU on just about any residential lot in Vancouver, behind just about any style of house.

The result? ADUs have proved *spectacularly* popular in Vancouver. As the [Sightline Institute documents](#), the city is adding more than 1,000 of these small, affordable homes every year, and the total share of single-family houses with legal ADUs is up to a staggering 35%.

What about Portland, on the other hand? Portland was an early adopter when it came to allowing ADU construction: they've been allowed as-of-right (i.e. without going through a lengthy petition process and public hearing) since 1997. However, very few were built up through 2010. Since 2011, [Kol Peterson documents](#), the number of permits per year has skyrocketed from under 100 to over 600. What changed in 2010 was simple: the city eliminated System Development Charges, a form of [development impact fee](#), for new ADUs, shaving thousands of dollars off the cost of creating one. Since then, Portland has also loosened design and size requirements for ADUs.

The biggest hurdles to building an ADU may be things that don't directly have to do with the zoning code. The ease or difficulty of financing, access to design professionals and contractors (or easily replicable templates), and the knowledge and confidence required to undertake a major project in your own backyard all have roles to play.

The next frontier for cities that really want to enable gentle, incremental development in their neighborhoods should be finding ways to help with these challenges. For example, [pre-approved templates](#) can take much of the uncertainty out of the permitting process. And novel business models can help with financing. A startup called Dweller [will build an ADU for you on your land](#) and take a cut of the monthly rent.

ADUs are a scalable, incremental response to high demand and high prices in cities. But this is only true if we allow the conditions for people to build them at scale: by the hundreds, not the dozens.

*(Cover photo: Radcliffe Dacanay via [Flickr](#).)*

## Research Summary of ADU Ordinances

Cities	Type of ADU	Use Approval	Min/Max Size of ADU	Bedrooms	Occupancy	Minimum Lot Size	Setbacks	Where Permitted	Utility Service	Parking Requirement	Addressing	Owner Occupancy	Architectural & Design Requirements	Home Occupation Regulations
<b>Apple Valley</b>	Attached or Internal	Conditional Use Permit	Min: 300 SF Max: 40% of principal unit	Max 2	Max 3	40,000 SF	Subject to existing zoning standards.	Lowest density single family	Requires connection, but does not specify how.	Two off-street parking spaces required.	N/A	Owner occupancy of one of the units. May not be subdivided.	Must be consistent with principal unit.	Must comply with existing regulations. Home occupation permitted in only one of the two units on the lot.
<b>Belle Plaine</b>	Attached or Internal	Interim Use Permit	Min: 300 SF Max: 40% of principal unit or 900 SF, whichever is less	N/A	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	Single family only	Same service as principal unit.	Two off-street parking spaces required.	If separate entrance is provided, unit should have same street address, with separate unit designation (i.e. XXX Main St. Unit A).	Owner occupancy of one of the units. May not be subdivided.	Must maintain appearance of principal unit.	N/A
<b>Bloomington</b>	Attached or Internal	Permitted by Right	Min: 300 SF Max: 960 SF	Max 2	Max 2	Subject to zoning district standards.	Subject to existing zoning standards.	Single family only	Same service as principal unit.	Must meet parking requirements for principal unit.	N/A	Owner occupancy of one of the units. May not be subdivided.	Must be designed and constructed to maintain the outward appearance of one single-family dwelling.	Must comply with existing regulations. If both units have home occupations, performance standards for a single unit must be met (i.e. drop offs/pick-ups, appointments, etc.).
<b>Burnsville</b>	Attached, Internal, or Detached	Permitted by Right	Min: 300 SF Max: 33% of principal unit or 960 SF, whichever is less	Max 2	N/A	10,000 SF for attached. 1 acre for detached.	Subject to existing zoning standards.	Single family districts	Same service as principal unit.	Two off-street parking spaces required.	N/A	Owner occupancy of one of the units. May not be subdivided.	Consistent with principal unit.	N/A
<b>Crystal</b>	Attached, Internal, or Detached	Permitted by Right	Min: N/A Max: 50% of principal unit	N/A	N/A	N/A	Subject to existing zoning standards.	Single family lots	Same service as principal unit or serviced separately, through new connection.	N/A	N/A	Both units can be rented.	Compatible with principal unit.	N/A

Cities	Type of ADU	Use Approval	Min/Max Size of ADU	Bedrooms	Occupancy	Minimum Lot Size	Setbacks	Where Permitted	Utility Service	Parking Requirement	Addressing	Owner Occupancy	Architectural & Design Requirements	Home Occupation Regulations
<b>Eagan</b>	Attached or Internal	Permitted by Right	Min: 300 SF Max: 33% of principal unit or 960 SF, whichever is less	Max 2	Max 2	N/A	Subject to existing zoning standards.	Low density single family districts	Same service as principal unit.	Two off-street parking spaces required.	N/A	Owner occupancy of one of the units. May not be subdivided.	Consistent with principal unit.	N/A
<b>Faribault</b>	Detached	Conditional Use Permit	Min: 300 SF Max: 864 SF	N/A	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	Higher density single family lots	Same service as principal unit.	Must meet parking requirements for principal unit.	If separate entrance is provided, unit should have same street address, with separate unit designation (i.e. XXX Main St. Unit A).	Owner occupancy of one of the units. May not be subdivided.	N/A	N/A
<b>Inver Grove Heights</b>	Attached, Internal, or Detached	Permitted by Right	Min: 250 SF Max: 1000 SF	N/A	Max 3	1 acre (for detached only). Otherwise subject to zoning district standards.	Subject to existing zoning standards.	Single family lots	N/A	Two off-street parking spaces required.	Separate address for detached unit only.	Owner occupancy of one of the units. May not be subdivided.	Exterior changes must not alter single-family character. Must match principal structure.	N/A
<b>Lakeville</b>	Internal only	Administrative Permit	N/A	N/A	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	Single family and two-family districts except CBD	Same service as principal unit.	3 stall garage at a minimum. If rented, one stall per renter required.	Same address as principal unit.	Can be rented. No more than 2 individuals who are unrelated to the principal family.	N/A	N/A
<b>Long Lake</b>	Attached, Internal, or Detached	Conditional Use Permit	Min: N/A Max: 900 SF	N/A	N/A	Twice the zoning district standard.	Subject to existing zoning standards.	Single family districts	Requires connection, but does not specify how.	Two off-street parking spaces required.	Same address as principal unit.	Owner occupancy of one of the units. May not be subdivided.	N/A	N/A

Cities	Type of ADU	Use Approval	Min/Max Size of ADU	Bedrooms	Occupancy	Minimum Lot Size	Setbacks	Where Permitted	Utility Service	Parking Requirement	Addressing	Owner Occupancy	Architectural & Design Requirements	Home Occupation Regulations
<b>Mahtomedi</b>	Attached, Internal, or Detached	Conditional Use Permit	Min: 300 SF Max: 33% of principal unit or 960 SF, whichever is less	Max 2	Max 2	10,000 SF	Subject to existing zoning standards.	Single family and agricultural districts	Same service as principal unit.	One off-street parking space required.	If separate entrance is provided, unit should have same street address, with separate unit designation (i.e. XXX Main St. Unit A).	Owner occupancy of one of the units. May not be subdivided.	Must be designed and constructed to maintain the appearance of one single-family dwelling.	Must comply with existing regulations. If both units have home occupations, performance standards for a single unit must be met (i.e. drop offs/pick-ups, appointments, etc.).
<b>Minnetonka</b>	Internal only	Permitted by Right	Min: N/A Max: 950 SF or 35% of principal unit, whichever is less	N/A	N/A	N/A	N/A	Single family districts	N/A	"adequate off-street parking provided"	N/A	Owner occupancy of one of the units.	Exterior changes must not alter single-family character.	N/A
<b>Plymouth</b>	Attached or Detached (above garage only)	Administrative Permit	Min: N/A Max: 1,000 SF or size of principal unit, whichever is less	N/A	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	New construction single family lots only	Requires connection, but does not specify how.	Two off-street parking spaces required.	Separate address pursuant to Building Code.	Owner occupancy of one of the units.	Similar to principal unit and compatible with character of neighborhood.	N/A
<b>Prior Lake</b>	Internal only	Permitted by Right	Min: N/A Max: 950 SF or 35% of principal unit, whichever is less	N/A	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	Residential districts	N/A	Two off-street parking spaces required.	N/A	Owner occupancy of one of the units. Other limited to family or caretakers.	Exterior changes must not alter single-family character.	N/A
<b>Richfield</b>	Attached, Internal, or Detached	Permitted by Right	Min: 300 SF Max: 800 SF or size of principal unit, whichever is less	N/A	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	Single family only	N/A	Three off-street parking spaces required.	N/A	Owner occupancy of one of the units. May not be subdivided.	Must match principal structure.	N/A

Cities	Type of ADU	Use Approval	Min/Max Size of ADU	Bedrooms	Occupancy	Minimum Lot Size	Setbacks	Where Permitted	Utility Service	Parking Requirement	Addressing	Owner Occupancy	Architectural & Design Requirements	Home Occupation Regulations
<b>Roseville</b>	Attached, Internal, or Detached	Permitted by Right	Min: 300 SF Max: 650 SF or 75% of principal unit, whichever is less	Max 1	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	Single family lots	N/A	One off-street parking spaces required.	If separate entrance is provided, unit should have same street address, with separate unit designation (i.e. XXX Main St. Unit A).	Owner occupancy of one of the units. May not be subdivided.	Exterior changes must not alter single-family character. Must match principal structure.	Must comply with existing regulations. If both units have home occupations, performance standards for a single unit must be met (i.e. drop offs/pick-ups, appointments, etc.).
<b>Shoreview</b>	Internal only	Permitted by Right	Min: 500 SF Max: 800 SF or 30% of principal unit, whichever is less	Max 2	N/A	Subject to zoning district standards.	Subject to existing zoning standards.	Single family lots	N/A	Three off-street parking spaces required, two of which must be enclosed.	Same address as principal unit.	Owner occupancy of one of the units. May not be subdivided.	N/A	N/A
<b>White Bear Lake</b>	Attached, Internal, or Detached (must be located in an already existing structure)	Conditional Use Permit	Min: 200 SF (plus 100 SF for each additional occupant) Max: 880 SF or 40% of principal unit, whichever is less	N/A	Max 4	Subject to zoning district standards.	Subject to existing zoning standards.	Single family lots	N/A	Additional parking may be required.	N/A	Owner occupancy of one of the units. May not be subdivided.	Any exterior modifications require approval from the City Council and may not detract from character of neighborhood. Must also match principal structure.	N/A



601 Main Street  
Elko New Market, MN 55054  
phone: 952-461-2777 fax: 952-461-2782

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## MEMORANDUM

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**TO:** PLANNING COMMISSION  
**FROM:** HALEY SEVENING, PLANNER I  
RENEE CHRISTIANSON, COMMUNITY DEVELOPMENT SPECIALIST  
**RE:** USE OF CITY DEVICES & SOCIAL MEDIA POLICY  
**DATE:** JULY 28, 2020

### **Background / History**

Each month, Staff includes a “Tip of the Month” in the Planning Commission Packet. These tips could range from information about City Ordinance or State Statute to educational materials about planning related topics. July’s “Tip of the Month” is information about the use of City devices and the City’s Social Media Policy.

With the recent distribution of City laptops for the Planning Commission, Staff would like to review the following City policies with the Planning Commission:

- Email & Internet Usage
- Electronic Communication Equipment
- Social Media

The main purpose of these policies are to provide standards to all City employees regarding authorized usage and procedures while using city-owned electronic communications equipment and ensure safe work practices when using these devices.

All questions and/or concerns related to use of city-owned electronic communications equipment (i.e. issues with hardware/software, clarification on City Policy, damage to/loss of device) should be directed to Renee Christianson at [rchristianson@ci.enm.mn.us](mailto:rchristianson@ci.enm.mn.us) or 952.461.2777.

### **Attachments**

- Email Policy
- Internet Use Policy
- Electronic Communication Equipment Policy
- Social Media Policy

USE OF CITY DEVICES & SOCIAL MEDIA POLICY

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July 28, 2020

## EMAIL

The City maintains an email system. The use of the email system is reserved for conducting City business. All messages composed, sent, or received on the email system are and remain the property of the City. They are not the private property of any employee. The confidentiality of any message should not be assumed. Even when a message is erased, it is still possible to retrieve and read that message. Further, the use of passwords for security does not guarantee confidentiality. E-mail messages should be drafted in the same manner and with the same care as any communication in printed form on the City letterhead.

The email system may not be used to solicit or proselytize for commercial ventures, religious or political causes, outside organizations, or other non-job related solicitations. The email system is not to be used to create, send, print or disseminate any offensive or disruptive messages. Among those which are considered offensive, are any messages which are pornographic or which contain sexual implications, racial slurs, gender-specific comments, discriminatory comments, or any other comment that offensively addresses someone's age, sexual orientation, religious or political beliefs, national origin, or disability. The email system shall not be used to send (upload) or receive (download) copyrighted materials, trade secrets, proprietary information or similar materials without prior authorization. The email system shall not be used for engaging in any activity in violation of local, state, or federal law. Employees shall not use a code, access a file, or retrieve any stored information, unless authorized to do so. Employees should not attempt to gain access to another employee's messages without the latter's permission.

The City has and will exercise the right to review, audit, intercept, access and disclose all messages created, received or sent over the email system for any purpose. The contents of the email properly obtained for legitimate business purposes, may be disclosed within the City without the permission of the employee. An employee should have no expectation of privacy in messages or files they create, send, read or listen to on City computers. Notwithstanding the City's right to retrieve and read any email messages, such messages should be treated as confidential by other employees and accessed only by the intended recipient. Employees are not authorized to retrieve or read any e-mail messages that are not sent to them. Any exception to this policy must receive prior approval by the Network Administrator.

## INTERNET USE

As an employee of the City of Elko New Market you are provided with access to the vast information resources of the Internet. The facilities to provide that access represent a considerable commitment of the City resources for telecommunications, networking, software, storage, etc. This Internet usage policy is designed to help you understand our expectations for the use of those resources in the particular conditions of the Internet, and to help you use those resources wisely.

Unnecessary or unauthorized internet usage causes network and server congestion. It slows other users, takes away from work time, and consumes supplies and ties up printers and other shared resources. Unlawful Internet usage may also garner negative publicity for the City and expose the City to significant legal liabilities. This policy applies to all users of City-owned-and-operated computer systems and networks. Any exceptions to this policy require the expressed written consent of the City. Any employee who discovers a violation of this policy shall immediately notify the City Administrator. Any employee who violates this policy or uses the Internet system for improper purposes shall be subject to discipline, up to and including discharge.

1. The use of the City's Internet is intended for City business, including research, communication and professional purposes within the business objectives of the City.
2. The City reserves the right to monitor and review all employee Internet usage. No employee should have any expectation of privacy as to his or her Internet usage.
3. The confidentiality of any electronic message using the City's Internet system should not be assumed. Even when a message is erased, it is still possible to retrieve and read that message.
4. Personal use of Internet service cannot interfere with business operations and should be limited to non-working hours.
5. Internet services, or any other network or computer resources, shall not be used for viewing, archiving, storage, distribution, editing or recording of threatening, obscene, harassing or derogatory material, or transmittal of material that is confidential to the City.
6. Internet services, or any other network or computer resources, shall not be used for the viewing, archiving, storage, distribution, editing or recording of any kind of sexually explicit image, material or document.
7. Use of the Internet system to receive (download) software programs, utilities or software extensions is prohibited without prior authorization from the Network Administrator. This includes, but is not limited to, screen savers, games and

utility programs. It does not include files such as Word documents, Excel documents, Adobe Portable Document Format (pdf) files and the like.

8. Any software or files downloaded via the Internet into the network become the property of the City. Any such files or software may be used only in ways that are consistent with their licenses or copyrights.
9. No employee may use City facilities knowingly to download or distribute pirated software or data. The use of file swapping software on City computers and City networks is prohibited.
10. No employee may use the City's Internet facilities to deliberately propagate any virus, worm or other illegal program code.
11. No employee may use the City's Internet facilities knowingly to disable or overload any computer system or network or to circumvent any system intended to protect the privacy or security of another user.
12. The City's Internet facilities and computer resources shall not be knowingly used to violate the laws and regulations of the United States or any other nation, or the laws and regulations of any state, city, province or other local jurisdiction in any material way. Use of any City resources for illegal activity is grounds for immediate dismissal, and the City will cooperate with any legitimate law enforcement activity.
13. Users of the City's information systems are prohibited from using password protection to restrict access to files on the City's systems without authorization from the City Administrator.
14. Each employee using the Internet facilities of the City shall identify himself or herself honestly, accurately and completely (including one's company affiliation and function where requested) when participating in chats or newsgroups or when setting up accounts on outside computer systems.
15. Anything an employee writes in email or on the Internet in the course of working for the City can be taken as representing the City's posture. For this reason, users of the City's Internet system are prohibited from using their City email address or otherwise identifying themselves as employees of the City when participating in non-work related online discussion forums, bulletin board, web sites or chat sessions.
16. Employees are reminded that chats and newsgroups are public forums where it is inappropriate to reveal private or confidential data. Employees releasing protected information via a newsgroup or chat, whether or not the release is inadvertent, will be subject to discipline.

17. Use of the City's Internet facilities to commit infractions such as misuse of City assets or resources, offensive behavior, illegal activity, unauthorized public speaking and misappropriation or theft of intellectual property are prohibited.

## **ELECTRONIC COMMUNICATION EQUIPMENT**

The main purpose of this policy is to provide standards to all City employees regarding authorized usage and procedures while using personal and city-owned or leased electronic communications equipment. It is also to ensure safe work practices when using city-owned or personal electronic communications equipment while driving city-owned vehicles or performing work-related activities. It is also intended to comply with applicable Federal and State laws related to the use of electronic communications equipment. Electronic communications equipment includes, but is not limited to, cellular phones, smart phones, Personal Digital Assistants and similar devices.

It is the goal of the City to prevent and correct any abuse or misuse of cellular phones through this policy. Employees who abuse or misuse electronic communications equipment in violation of this policy may be subject to disciplinary action. The City Administrator, or designee, will have the primary responsibility for implementation of this policy. All Department Heads and Supervisors will be responsible for enforcement within their departments.

### **City-Owned or Leased Electronic Communications Equipment**

1. Employees shall use their City-owned or leased electronic communications equipment in a cost-effective and responsible manner for business purposes. Electronic communications equipment is not secure, so employees should use discretion in relaying confidential information.
2. The equipment is authorized for official City business-only at all times.
3. Employees are prohibited from using either personal or City provided cellular telephones in violation of Minnesota Statutes sections 169.011 subdivision 94 and 169.475 concerning wireless communications devices and use of wireless communications devices while in the performance of City business.
4. Employees are not to access/download online features or applications, such as ringtones, available on this equipment without supervisory approval.
5. Employees in possession of this equipment are required to care for it in a responsible manner and to take appropriate precautions to prevent theft, damage and vandalism. Loss or theft should be reported immediately to their department head or supervisor. Upon the City's request, or upon leaving employment, an employee is responsible for returning electronic communications equipment to their department head or supervisor.
6. Random and periodic audits of bills may be made at the City Administrator's discretion.
7. A vehicle operator is allowed to use a cellular telephone only by voice commands or single-touch activation without holding the phone while operating a City or personal vehicle being used for City business, except as otherwise authorized under Minn. Stat. 169.475.
8. Voice and single-touch activation may be achieved through paring the cellular telephone to the vehicle, use of single earphone with a microphone either paired through a Bluetooth connection or connected by wire to the cellular telephone, or the use of a Bluetooth speaker with microphone paired to the cellular telephone.

9. Electronic communications equipment use in violation of State and Federal laws and City policy is prohibited. Employees must abide by State law regarding the use of electronic communications equipment while operating a city-owned vehicle.



## **CITY OF ELKO NEW MARKET SOCIAL MEDIA POLICY**

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### Overview

The City of Elko New Market will use social media websites as additional communication channels to promote the City's programs, events, and services, and represent itself appropriately and consistently on the Internet. The City's official website, located at [www.ci.enm.mn.us](http://www.ci.enm.mn.us) shall remain the City's primary online medium, along with City newsletter and City Digital Sign for communicating information to the public.

A social media presence is an additional way government can communicate to the broadest number of residents it serves. The City of Elko New Market's goal is to increase transparency, immediacy, and feedback to the community and enable residents obtain information online.

The best, most appropriate, City of Elko New Market uses of social media tools fall into two categories:

- Channels for disseminating time-sensitive information as quickly as possible (i.e. emergency information)
- As marketing/promotional channels that increase the City's ability to broadcast its messages to the greatest number of residents.

### 1. Purpose

It is the intent of the City of Elko New Market to represent itself appropriately, consistently and positively on the internet. The purpose of this policy is to establish procedures for creating and maintaining a consistent overall social media presence which meets the City's Community Oriented Local Government Philosophy and Vision Statement.

The City of Elko New Market will determine, at its discretion, how its web-based social media and online community presence will be designed, implemented and managed as part of its overall communications and information technology strategies and functions. City government social media and other web-based resources may be modified or removed by the City at any time and without notice, as necessary to maintain the integrity of both communications and information technology functions. This policy establishes guidelines for the use of social media by individual City departments, City employees and City elected or appointed officials.

### 3. Scope

This policy applies to any existing, as well as pre-existing or proposed, social media networking accounts sponsored by City departments. The policy also applies to the posting information on behalf of the City on non-City social media websites.

### 4. Definitions

**“City Social Media”** – Those social media web sites established for the benefit of the City of Elko New Market with authority granted as outlined in this policy. This also includes social media accounts as approved by City management. City Social Media does not include employee’s personal social media accounts.

**“Social Media Web Sites”** –Web site that enables users to create public profiles within that Web site and form relationships with other users of the same Web site who access their profile. Social networking sites can be used to describe community-based Web sites, online discussions forums, chatrooms and other social spaces online.

**“Social Networking”** – Grouping of individuals into specific groups and offers a way for registered users to communicate with each other on the internet.

### 5. General Standards

The City Administrator and his/her designee is responsible for administering City social media accounts and coordinating the posting of information on behalf of the City on non-City social media websites.

The City of Elko New Market social media accounts and posting of information on behalf of the City on other social media sites, along with associated content will focus on significant City interest areas, such as City events, activities or services, etc. and be organized in a manner that avoids ambiguities and/or conflicting information across the City’s various communication mediums, including other City social media websites. Departments’ administration and use of City social media websites must comply with applicable laws, regulations and policies.

Social media websites are to be consistently branded in order to communicate a clear association with the City, and must contain a City standard disclaimer. Branding should include, but is not limited to, the inclusion of the City logo, city department information (address, phone number, and hours of operations) and any other aesthetic “look and feel” defined by the City Administrator or his/her designee.

The naming and contact conventions used for social media accounts should be City of Elko New Market-specific and must not contain individual employee names, with the exception of professional staff social media accounts.

## 6. Staff Members Maintaining City Social Media Sites

- City social media messages may not be used by any City employee or representative for private or personal purposes or for the purpose of expressing private or personal views on personal, political or policy issues.
- City social media messages may not be used to express personal views or concerns pertaining to City employment relations matters.
- For the purposes of efficiency, expediency and consistency, Elko New Market's social media outreach will be centralized to the City Administrator or his/her designee. All postings to City of Elko New Market social media site may be subjected to review by the City Administrator and his/her designee(s) and may be deleted without notice. Staff members assigned the task of maintaining social media sites on behalf of the City must follow this Policy and all relevant sections of the City's Personnel Policy.
- Employees representing the City of Elko New Market on the City's and other social media sites must conduct themselves professionally at all times as representatives of the City.
- Departments are responsible for forwarding requests to the City Administrator and his/her designee(s) to make social media posts.
- Administration of all social media web sites and online community accounts must comply with applicable laws, regulations and policies.
- City of Elko New Market social media and online community web site accounts are considered a City asset and logins to these accounts must be securely administered in accordance with City computer security policies. **The City reserves the right to shut down any of its social media sites or accounts for any reason without notice.**
- A social media web site shall not be used by the City or any City employee or representative to disclose private or confidential information without the prior written approval of the City Administrator.
- All social media web sites and online community accounts created and utilized during the course and scope of an employee's performance of his/her job duties will be identified as belonging to the City of Elko New Market, including the City logo, as well as a link to the City's official web site.

- Inclusion of the following disclaimer, as approved by the City Administrator, is required on all social media sites:

**“The City of Elko New Market’s use of external social media web sites is provided as a public service. The City of Elko New Market disclaims liability for ads, videos, promoted content or comments accessible from any external web site. The responsibility for external content rests with the organizations or individuals providing them. Any inclusion of external content does not imply endorsement by the City of Elko New Market. The City reserves the right to delete any postings that are obscene, vulgar, threatening, contain profanities or show the City or its residents in a negative manner.”**

### City Social Media Posts

The City’s official website shall remain the City’s primary online medium for communicating information to the public. Whenever practical, content posted to social media sites shall contain links directing users back to the City’s official website for in-depth information, forms, documents, or online services necessary to conduct business with the City of Elko New Market.

### Accuracy and Timeliness

The City of Elko New Market will make every effort to ensure the accuracy of the information provided on its social media pages. However, several factors that are beyond the City’s control (including unauthorized modification of electronic data, transmission errors, browser incompatibilities, information that has been cached on the local computer or storage device, or other aspects of electronic communication in an evolving and time-sensitive environment) can affect the quality of the information displayed on the site. For that reason, the City does not guarantee the accuracy of the information provided on its social media page and is not liable for reliance on this information.

The City’s timeliness about posts may also vary based on staff availability.

Therefore, a disclaimer shall be posted on any social media site, stating:

*This social media page is intended to provide information timely to a wide number of people, not as an in-depth or complete source of information, but as a supplement to the City of Elko New Market’s website (www.ci.enm.mn.us, newsletter, press releases, and in-person communications. Staff will post within office hours (Monday-Friday, 8 am-4:30 pm) and within the varying parameters of staff availability.*

### Employee’s Personal Social Media Pages

The line between personal and professional, public and private can be easily blurred in social media. Keep the following guidelines in mind when using a personal social media account:

- a) Personal social media account names by employees should not be tied to the City. Additionally, Staff should not use their personal social media accounts to speak on behalf of the City.
- b) City Staff members have the right to speak publicly as a private citizen on matters of public concern. However, employees are reminded to use good judgment when commenting on a City social media site or if commenting or posting on a private site regarding a City related issue, policy, etc. If you are identifying yourself as a City employee, your actions reflect upon the City.
- c) Employees with personal social media accounts DO NOT have the right to post non-public and confidential information such as information related to co-workers, personnel data, medical information, and claims or lawsuits against the City.
- d) Video or pictures obtained by employees on duty are the property of the City. Employees shall not post those pictures or videos on social media sites without approval from the City Administrator and his/her designee(s).

#### Social Media Guidelines For Elected and Appointed Officials

Elected officials should comply with the following guidelines when using City of Elko New Market social media sites:

- Elected officials should comply with the guidelines outlined in the City's Social Media Policy.
- Elected officials shall not use official City social media sites for campaigning purposes.
- Elected officials shall not post comments or links on City social media to any content that endorses or opposes political candidates or ballot propositions, including links to an elected official's campaign site.
- ALL comments posted on City social media during an election season by anyone who has filed for office will be removed.
- Elected officials must be aware of the risks of electronic communication in relation to the Minnesota Data Practices Act and the Open Meeting Law; two-way communication between elected officials should be strictly avoided.
- Elected officials should not use social media as a mechanism for conducting official City business other than to informally communicate with the public and direct the public to City information resources (e.g. City social media, City website, staff, consultants). Elected officials may comment or provide statements of positions on broader policy issues, but may not make any comments demonstrating bias on issues that will come before the council the future for a quasi-judicial decision.

- Elected officials should reveal that they are elected officials for the City if/when making a post and be honest, straightforward, and respectful.
- Elected officials should be sure that efforts to be honest don't result in sharing non-public information related to coworkers, personnel data, medical information, claims or lawsuits, or other non-public or confidential information.
- To help prevent errors and reduce potential liability, elected officials should not post official information about the City and should make sure that any comments they make on social media are factually correct. However, elected officials may and are encouraged to provide references to official sources of information (e.g. staff, consultants, links to City social media and City website).
- If an elected official makes a mistake, it should be corrected as soon as the official is made aware of the error. Corrections should be upfront and as timely as possible. If you modify an earlier post, make it clear the posting has been corrected. Consider designating corrections with "Fixed link" or "Fact correction" prior to the correction.

#### 8. Violations of the Policy

Violations of this Policy shall be reported to the City Administrator and his/her designee(s). Employees violating this policy may be disciplined in accordance with the provisions of the City of Elko New Market' Personnel Policy or employee's labor contract.

#### 9. Exceptions/Changes

This policy replaces all previous policies covering the same or similar topics except as provided for in the City of Elko New Market Personnel Policy. Exceptions to this policy may be granted only by the City Administrator. This policy may be reviewed and changed at any time.